



Joint Development Authority
TCAAP Redevelopment Project



SPECIAL JDA MEETING AGENDA
Tuesday, March 10, 2026, 5:30pm at Arden Hills City Hall

1. Roll Call
2. Approval of Agenda
3. Approval of Minutes
4. Public Input
5. Consent Agenda
6. Old Business
7. Public Hearings
8. New Business
 - a. Consider TRC Amendment Requests
 - b. Review Annual Report
 - c. Review Road Map
9. Administrative Director's Report
10. Development Director's Report
11. Commissioner Updates
12. Adjournment



Joint Development Authority
TCAAP Redevelopment Project



AGENDA ITEM 2

MEMORANDUM

DATE: March 10, 2026
TO: Joint Development Authority Board of Commissioners
FROM: Directors Jagoe and Mitchell
SUBJECT: Approval of Agenda

The agenda for the March 10, 2026, Special JDA Meeting is presented for approval.

Action Requested:

Approve the agenda for March 10, 2026.



Joint Development Authority
TCAAP Redevelopment Project



AGENDA ITEM 3

MEMORANDUM

DATE: March 10, 2026

TO: Joint Development Authority Board of Commissioners

FROM: Directors Jagoe and Mitchell

SUBJECT: Approval of Minutes

The minutes for previous JDA meetings are presented for approval. On December 1, there were three meetings held in succession – a Special Closed Meeting, a Work Session, and a Special JDA Meeting – however only the minutes from the latter two were approved at the previous JDA meeting. The minutes from the first of these, the Special Closed Meeting, is included now.

Attachments:

- December 1 Special Closed JDA Meeting Minutes
- February 2 JDA Work Session Minutes
- February 2 Special JDA Meeting Minutes

Action Requested:

Approve the minutes from December 1, 2025 and February 2, 2026.

Joint Development Authority
Closed Meeting
Monday, December 1, 2025
Arden Hills City Hall
(upstairs conference room)

Minutes

4:00 pm

Roll Call

Joint Development Authority: Chair Jon Wicklund, Commissioner Tara Jebens-Singh, Commissioner Kurt Weber, Commissioner Mary Jo McGuire (virtual), Commissioner Tena Monson

Also present: Jessica Jagoe (Arden Hills); Ella Mitchell (Ramsey County), Rachel Tierney (Kennedy & Graven); Jenny Boulton (Kutak Rock), Kathleen Ritter (Ramsey County, virtual), Kori Land (Levander), James Thomson (Kennedy & Graven), Michelle Weinberg (Kennedy & Graven)

Roll call taken.

The TCAAP Joint Development Authority Board of Commissioners (JDA) held a special JDA meeting pursuant to Minnesota Statutes, Section 13D.04, subdivision 2, on Monday, December 1, 2025, commencing at 4:00 p.m., at Arden Hills City Hall.

Closed Meeting

Motion by Commissioner Weber seconded by Commissioner Monson to go into closed session McGuire to go into closed session pursuant to Minnesota Statutes, section 13D.05, subdivision 3(b) regarding pending litigation in Alatus Arden Hills LLC vs TCAAP Joint Development Authority. Motion carried at 4:00pm.

Discussion of litigation.

Motion by Commissioner Weber seconded by Commissioner Monson to reopen the meeting. Motion carried at 5:00 pm.

Adjournment

Motion by Commissioner Weber to adjourn seconded by Commissioner Jebens-Singh. Meeting adjourned at 5:01 pm.

Approved _____
Jon Wicklund, Chair

Date

Joint Development Authority Work Session
Monday, February 2, 2026
Arden Hills City Council Chambers

Minutes

5:30 pm

Roll Call

Joint Development Authority: Chair Jon Wicklund, Commissioner Tara Jebens-Singh, Commissioner Kurt Weber, Commissioner Kelly Miller, Commissioner Tena Monson

Also present: Jessica Jagoe (Arden Hills); Ella Mitchell (Ramsey County), Rachel Tierney (Kennedy & Graven), John Mazzitello (Ramsey County), Eric Fosmo (Kimley Horn)

Roll call taken.

Public Input

None.

New Business

a. Discuss Infrastructure Update

Eric Fosmo, Kimley Horn, stated at the October 2025 JDA work session, staff presented an infrastructure project update, outlining the overall scope, progress, and key design considerations. During that presentation and work session discussion, JDA members raised several questions and provided feedback that identified areas requiring additional analysis and follow-up by both staff and the project design team.

Mr. Fosmo reported since that time, staff and the design team have reviewed the comments received, evaluated potential options, and refined elements of the project in response to the JDA's discussion. Staff provided an updated presentation on the infrastructure project, beginning with a PowerPoint presentation summarizing project status, and outstanding considerations. As part of this update, staff returned to two specific topics that generated significant discussion in October: lighting and trail design. These items were brought back to the JDA for further discussion and direction to ensure alignment with project goals, the TRC, and the overall design intent.

Mr. Fosmo explained in addition to the presentation, a technical memorandum was provided for JDA review. The technical memorandum discusses both the lighting and trail design elements as outlined in the TCAAP Redevelopment Code (TRC) and provides supporting detail related to the proposed infrastructure improvements. He reviewed a typical roadway section with utilities and commented on where district energy could be located. He noted the City's

logo or name could be added to the Rice Creek Boulevard bridge railing. Coordination efforts with the US Army were discussed and it was noted coordination continues to be challenging. He commented on the potential 2026 site preparation work that would be done. The TRC Amendment Item #1 which addresses the multi-use trail width was reviewed. He provided further information on the site preparation and grading plans within Rice Creek Commons and asked for comments or questions from the JDA.

Chair Wicklund asked if the JDA had the flexibility to make some decisions when it came to the TRC. He questioned what the JDA was reviewing in this situation. Director Jagoe explained specific requirements, when it comes to trails and lighting would need an adjustment. She indicated the TRC would require an amendment if this was the direction from the JDA.

Commissioner Monson stated there were timing impacts and asked what the delay would be to go back to the watershed district. Ramsey County Public Works Director John Mazzitello anticipated this would take six months. He commented there would also be delays while the County continues to coordinate with the US Army. Mr. Fosmo reported he did not believe a change to the trail width would delay construction starting in 2027.

Commissioner Monson inquired how long it would take to complete the ordinance change with the City. Director Jagoe anticipated this would take three months.

Commissioner Weber thanked staff for coming back to the JDA with responses to the questions that were raised last October. He stated he could support moving to a 10-foot trail, especially because there would be trails on both sides of the road. He commented further on how the reduction in the trail width would improve the stormwater management situation for the entire project.

Commissioner Jebens-Singh asked what the rationale was for the 12-foot trail width in the TRC. Director Jagoe explained she has not located anything in the files as to why a 12-foot trail width was recommended, but noted she would be addressing this topic with the City Council. Mr. Fosmo reported standard trail width in Arden Hills, not within RCC, was 8 feet.

Commissioner Weber inquired if the TRC amendment for the trail width would be requested by the County or the City. He noted this change would apply to a County Road and perhaps the request should be coming from the County. Director Jagoe indicated she was looking for direction from the JDA to direct the City to consider an amendment to the TRC.

Commissioner Weber stated the reason he asked this question was because the Planning Commission may ask questions and a significant amount of work may go into the request. Public Works Director Mazzitello reported his design team would prepare the plans within the application package.

Chair Wicklund encouraged staff to further consider who would be submitting the TRC amendment request. Public Works Director Mazzitello indicated it was the County's desire to have a collaborative application from the City, County and JDA.

Commissioner Monson inquired why the plans were designed with 10-foot trails when the TRC calls for 12-foot trails. Public Works Director Mazzitello explained there were only two outliers and noted the County's requirement was followed for this trail because the trail would be located on County right of way.

Commissioner Monson explained the TRC was completed through coordination between the City and County. She cautioned the County from designing plans that meet County requirements and not the TRC requirements.

The consensus of the JDA was to support a 10-foot trailway width within RCC.

Chair Wicklund asked that the discussion now focus on the lighting requirements.

Mr. Fosmo reviewed the TRC Amendment Item #2 which addressed street lighting spacing. He stated the 40-foot light spacing requirement was cost prohibitive. He noted typical spacing was 150 to 200 feet. He explained the typical lighting requirements from MNDOT were 210 to 240 feet. He discussed how lighting was spaced along Rice Creek Parkway and County Road E/Snelling Avenue noting the distance for lighting was 140 to 200 feet.

Commissioner Weber inquired if a photometric survey or modeling was completed. He indicated he was concerned with the fact the lights would be 40 feet apart and recommended the spacing be larger. He reported he would like to see different distances modeled to see if there were any dark spots prior to moving forward with a TRC amendment for street lighting.

Commissioner Miller asked what the County's typical light spacing standards were. She inquired if the trail would be owned by the City or County. Public Works Director Mazzitello stated as a general rule the County does not light County roads but rather lights intersections. He explained the trail would be owned by the County because the trail would be located within County right of way but the City would be responsible for the trail maintenance. He clarified the City would be the owner and operator of the lights.

Commissioner Miller inquired if the County would be receiving additional requests to have trails lit if the trail within RCC were lit by the County.

Commissioner Weber indicated the County may not typically light its trails, but the TRC was agreed upon by the City and County and he would like to see the proposed design standards followed. He requested the photometric survey be created for the spine road with the lighting at 40 feet and other distances in order for the JDA to better understand the proposed changes.

Public Works Director Mazzitello stated he was not seeking a specific distance at this time, but rather was seeking greater flexibility in order to pursue a context-appropriate lighting plan.

Commissioner Monson anticipated this was very important language to the City given how the County does not light any of its other streets. She understood the language could be made more reasonable, but the City was very specific with the lighting requirements for a reason.

The consensus of the JDA was to request the County to complete a photometric plan that includes trail lighting with context appropriate language that the JDA could react to at a future meeting.

Commissioner Jebens-Singh asked if this project could move forward without action from the US Army. Public Works Director Mazzitello reported the project could not move forward until coordination has been completed with the US Army.

Commissioner Miller questioned who would pay for the relocation of the TGRS system. Public Works Director Mazzitello explained this was included in the estimated costs for site preparation.

Commissioner Weber asked what information the County needs from the US Army. Mr. Fosmo stated he was seeking information on the design details of the TGRS system and he needed to know what the US Army wants out of this system going forward. He discussed how work and communication with the Army's consultant has been delayed since the government shutdown. Public Works Director Mazzitello indicated the Army was very aware of the information the County has requested.

Chair Wicklund stated it appears this project could be stalled without further action from the US Army. Public Works Director Mazzitello stated that per a clause in the property title the Army cannot impede development of the property.

Chair Wicklund recommended the strategy with the US Army be well thought thru to ensure no gaslighting occurred.

Commissioner Jebens-Singh suggested the Advisory Team discuss this topic.

Chair Wicklund supported this recommendation.

b. Discuss Hybrid Developer Approach

Director Mitchell stated at the December work session, the JDA reviewed a summary of responses to the Request for Information and asked staff for more information on a so-called "hybrid" developer approach. Staff and consultants have started to analyze the implications of this approach in comparison to the previous approach of working with a single lead developer

for the entire site. This memo builds on previous information provided and identifies next steps if the JDA would like to continue to explore this approach.

Director Mitchell explained the concept of a hybrid developer approach would be to divide the site into development areas and seek development partners through separate competitive solicitation processes for each area based on development expertise. For example, a solicitation for a neighborhood with residential land uses would seek a developer with residential expertise, and a neighborhood with commercial land uses would seek a developer with commercial expertise. It is possible that selected developers would bring on sub-developers to within development area to complement their expertise and add capacity.

Director Mitchell reported the JDA, City, and County may need to play a more active role in coordination amongst developers and other project partners to ensure infrastructure, amenities, and other sitewide items are covered. Ramsey County's scope and timeline for site preparation and construction of Rice Creek Boulevard would also be important to establish. Staff provided a summary of RFI responses on the development approach from the December JDA agenda packet. It is intended to serve as a starting point for discussion and is not necessarily an exhaustive accounting of all of the factors to consider.

Director Mitchell commented if the JDA would like to pursue to a hybrid developer approach, the next steps would be for the City and County staff to identify and analyze the items needed to be addressed in a cooperative agreement and offer options or solutions for each item to present to the respective parties. Staff estimate they may need 90-120 days to facilitate these conversations with their respective leadership and elected officials. The City and County would then report back to the JDA.

Commissioner Jebens-Singh stated she supported bringing on a real estate development consultant. She indicated if a phased project could bring about new funding sources, she supported looking into this option further.

Commissioner Monson asked what issue was being solved through the hybrid approach. Director Mitchell explained this approach may allow certain portions of the project to move forward faster. She indicated different developers and expertise could be brought in and the risk for this project would be diversified.

Commissioner Monson stated this did not fully answer her question noting the project would not be able to move forward with out coordination with the US Army. She commented the JDA has tried to approach this project with one lead developer. She indicated there were a lot of hurdles around the purchase agreement. Director Mitchell discussed how the purchase agreement had to be ironclad if the entire parcel was going to be sold to one developer. She indicated part of the risk diversification was to allow parcels to be sold off and this would create more simple purchase agreement negotiations.

Commissioner Monson summarized it appears the size of this project was really tough and it was hard to find a single developer to complete this project.

Commissioner Jebens-Singh stated after sitting in on negotiations last year, the expertise of any one developer was in question. She explained if the project were more broken up the County would be able to have money to reinvest back into the project.

Commissioner Weber anticipated the City would have a hard time with the site being sold in numerous parcels. While he understood this has benefits, he feared the final product could be compromised. He stated there may be parcels remaining within the project area that stay undeveloped because they are less desirable. He questioned if there would be coordination between the City and the County on the land sale negotiations. Director Jagoe indicated a cooperative agreement would have to be in place to address parks, infrastructure, phasing and the repayment of costs.

Chair Wicklund stated it would be his preference to have staff research the hybrid approach and consider how the California parcel could be split into five to ten sections. He indicated if the JDA were to proceed in this manner, parcels could begin to be sold off and if this wasn't working the JDA could move back to the lead developer scenario.

Commissioner Weber explained he generally agreed with this recommendation but he stated there were other factors to consider such as which parcels were the most developable.

Commissioner Miller supported the JDA pursuing a hybrid model for the development of Rice Creek Commons.

Commissioner Jebens-Singh encouraged staff to consider what the JDA was not thinking about if the hybrid model were to move forward. She indicated staff would have to consider what was the most logical manner in which to move forward with the development of this project and how it should be staged or phased.

Commissioner Monson stated she was not sold on the hybrid approach. She was of the opinion it was dangerous to ask the County to do development work given the fact that they were a governing body.

Chair Wicklund indicated this was why he would like to allow staff the time to research the options for a hybrid approach.

Commissioner Jebens-Singh recalled that the hybrid approach was brought up by a Commissioner for consideration. She supported the JDA learning more about this option and if there were any red flags with the hybrid approach.

The consensus of the JDA was to direct staff to pursue a hybrid approach and to report back to the JDA in 90 days.

Adjournment

Meeting adjourned at 7:07 pm.

Approved _____
Jon Wicklund, Chair

Date

Special Joint Development Authority
Monday, February 2, 2026
Arden Hills City Council Chambers

Minutes

7:15 p.m.

Roll Call

Joint Development Authority: Chair Jon Wicklund, Commissioner Tara Jebens-Singh, Commissioner Kurt Weber, Commissioner Kelly Miller, Commissioner Tena Monson

Also present: Jessica Jagoe (Arden Hills); Ella Mitchell (Ramsey County), Rachel Tierney (Kennedy & Graven)

Roll call taken.

Approval of Agenda

Motion by Commissioner Miller seconded by Commissioner Weber to approve the agenda as presented. Motion carried.

Approval of Minutes

Motion by Commissioner Weber seconded by Commissioner Jebens-Singh to approve the December 1, 2025 JDA Worksession, December 1, 2025 JDA, and December 22, 2025 Special Closed JDA meeting minutes as presented. Motion carried.

Public Input

Rich Strauman, 4380 Arden View Court, commented he appreciated the discussion the JDA had on trails and lighting within the TCAAP redevelopment project. He explained the residents of Arden Hills were excited about getting a trail on the west side of Snelling Avenue because they cannot get out of their community without driving. He questioned why the TRC recommended 12-foot trails. He suggested a 10-foot trail be pursued. He recommended the JDA further consider the side roads within the development. He asked that the roads within the development be well maintained and that the trails be properly cleared in the winter and well-lit for safety purposes.

Consent Agenda

None.

Old Business

None.

Public Hearings

None.

New Business

a. Elect Chair and Vice Chair

Director Mitchell requested the JDA elect a Chair and Vice Chair for 2026.

Motion by Commissioner Monson seconded by Commissioner Miller to elect John Wicklund Chair of the JDA for 2026. Motion carried.

Motion by Commissioner Monson seconded by Commissioner Miller to elect Tara Jebens-Singh Vice Chair of the JDA for 2026. Motion carried.

b. Review Road Map

Director Mitchell reviewed the Road Map with the JDA. She explained she was working with Goff on the JDA annual report and noted this would be done prior to the February 15 deadline. She indicated staff could work on an infrastructure webinar as previously discussed for March of 2026.

Commissioner Jebens-Singh requested staff take into consideration the comments that were made regarding trails and lighting and that these topics be written into the road map. She also suggested the infrastructure webinar be moved to allow time for these questions to be resolved.

Chair Wicklund recommended on May 4 the hybrid model report be presented to the JDA.

Chair Wicklund asked when the JDA would meet in September. Director Mitchell explained the tentative meeting date was Tuesday, September 8.

Administrative Director's Report

Director Mitchell stated sustainability work was ongoing and noted the staff was working on precertification and certification items. She noted locations for the potential geothermal well were also being considered at this time. She indicated staff has no new information on the Xcel Energy pilot program, except that Xcel has brought a consultant on board.

Development Director's Report

Director Jagoe explained she had no updates for the JDA.

Commissioner Updates

None.

Adjournment

Meeting adjourned at 7:32 pm.

Approved _____
Jon Wicklund, Chair

_____ Date



Joint Development Authority
TCAAP Redevelopment Project



AGENDA ITEM 4

MEMORANDUM

DATE: March 10, 2026

TO: Joint Development Authority Board of Commissioners

FROM: Directors Mitchell and Jagoe

SUBJECT: Public Input

The public is invited to provide input. Comments will be limited to three minutes per person.



Joint Development Authority
TCAAP Redevelopment Project



AGENDA ITEM 8a

MEMORANDUM

DATE: March 10, 2026

TO: Joint Development Authority Board of Commissioners

FROM: Directors Mitchell and Jagoe

SUBJECT: Consider Recommendation of TRC Amendment Requests

At the October 2025 JDA work session, staff presented an infrastructure project update focused on the planned Ramsey County infrastructure work. They outlined the overall scope, progress, and key design considerations and received feedback and questions from the JDA. Ramsey County staff and consultants came back to the February 2025 JDA work session with more information on these topics, including an updated project status and refined information on TCAAP Redevelopment Code (TRC) amendment requests. At that meeting, the JDA reached consensus around the direction on the topic of multi-use trail design width, however they asked for more information to support additional discussion on the topic of lighting.

A technical memorandum was provided at the February 2026 JDA work session, and it has been updated with the requested information and attached for JDA review. It discusses both the lighting and multi-use trail design elements as outlined in the TCAAP Redevelopment Code (TRC) and provides supporting detail related to the proposed infrastructure improvements.

Staff request that the JDA consider recommending that City and County staff bring the two TRC amendments requested by Ramsey County to their respective elected bodies for consideration. The first amendment request is for Multi-Use Trails along Rice Creek Boulevard and then second is for Street Lighting along Rice Creek Boulevard.

Attachments:

- Kimley Horn Memo
- Presentation

Action Requested:

Consider recommending that staff bring forward the two requested TCAAP Redevelopment Code (TRC) amendments to the City of Arden Hills and Ramsey County for their consideration.

RCC Proposed Amendments - TCAAP Redevelopment Code

Date: March 6, 2026
 To: John Mazzitello, Larina DeWalt – Ramsey County
 From: Eric Fosmo, Michael Kirsch – Kimley Horn

Executive Summary

The City of Arden Hills City Council approved the TCAAP Redevelopment Code (TRC) on July 13, 2015, with the most recent amendment (Amended Ord. 2024-002) approved on April 8, 2024. The TRC establishes requirements for the development of the Rice Creek Commons, including the public infrastructure that will support the development. Section 3.3.d of the TRC summarizes the portions of the TRC (Table 3-3) that the Joint Development Authority (JDA) is authorized to approve ‘Permitted Adjustments’.

The Rice Creek Commons (fka TCAAP) Public Infrastructure Improvements team has reviewed the TRC and is requesting an amendment to two (2) design elements where the preferred design alternative will not meet the TRC written code *AND* Section 3.3.d does not describe them as “Permitted Adjustments”. This memorandum provides a summary of these items and technical background detailing the justification for the requested variance/amendment.

The project team has modified the requested amendment language provided in this memo based on feedback from the February JDA work session meeting. Supporting documents for the lighting photometrics and design criteria are included in **Appendices A-C** attached to this memo.

Item #1: Multi-Use Trails along Rice Creek Boulevard (fka Spine Road)

Introduction Statement: The TRC code recommends a minimum 12-foot width for multi-use trails. Previous designs (2019 plans), Ramsey County standards/precedence, and City standards/precedence point to multi-use trails at a 10-foot width. The Rice Creek Watershed District’s treatment exemption for multi-use trails and sidewalks is also capped at a 10-foot width which is the principal catalyst for the requested amendment.

Definitions

Definitions of sidewalk vs Multi-Use Trail vs Shared Use Path:

Shared Use Paths and Multi-Use Trails are interchangeable terminology in the industry and are intended for multimodal uses and serve a variety of non-motorized uses. Sidewalks are intended for the use of pedestrians. (MN Statute 169.01 Subd. 75)

Current Applicable Code:

City of Arden Hills Code of Ordinances – Section 1380 Attachment #2 Palette of Open Space Types, Table “Typical Characteristics”

“g) Multi-use trail along Spine Road or through neighborhoods: Paved trail with frequent gathering spaces and regular landscaping. Standards - Min. Width 12 feet”

Amended Applicable Code (Proposed):

City of Arden Hills Code of Ordinances – Section 1380 Attachment #2 Palette of Open Space Types, Table “Typical Characteristics”

“g) Multi-use trail through neighborhoods: Paved trail with frequent gathering spaces and regular landscaping. Standards - Min. Width 12 feet”

Multi-use trail along Spine Road (Rice Creek Boulevard): Paved trail with frequent gathering spaces and regular landscaping. Standards - Min. Width 10 feet”

Practical Difficulty of meeting TRC (Current Applicable Code):

Rice Creek Watershed District Requirements:

The Rice Creek Watershed District Rules provide an exception which excludes stormwater treatment volume requirements for any trails that are 10 ft or less that are also bordered by down-gradient vegetation or vegetated filter strip of at least 5 ft (*Rule C Stormwater Management, 12 Exceptions (b)*). The Rice Creek Boulevard right-of-way contains adequate space for a vegetated filter strip for the project to take advantage of this exception.

Construction of 12-foot trails would require the project to apply the regular treatment standards as described Under Rule 6 of the Watershed District Rules for the entirety of the 12-feet of impervious surface created by the trail. The requirement stipulates 1.1-inches of treatment volume over new impervious surfaces. This results in additional 1,100 cubic yards of required stormwater treatment volume.

Cost Implications:

The construction of the additional trail width results in additional project construction costs for the additional trail width and stormwater treatment volume. Sufficient space does exist within the Natural Resources Corridor to provide the treatment volume; however, it does reduce capacity for future developments within Rice Creek Commons. A summary of those impacts is provided below:

Estimated Cost for Additional Trail Width: \$500,000

Estimated Cost for Additional Stormwater Treatment: \$125,000

Additional Treatment Volume Required: 1,100 CY

Acreage needed for Additional Treatment Volume: 0.40 Acres

Design Standards/Justification for Proposed Code Amendment:

County Precedence:

Ramsey County has a general precedence that shared use paths and trails are built to a standard width of 8ft when running adjacent to County roadways and 10ft where disconnected from adjacent roadways. The adjacent ‘Rice Creek North Regional Trail’ is constructed to a 10-foot width.

City Standards:

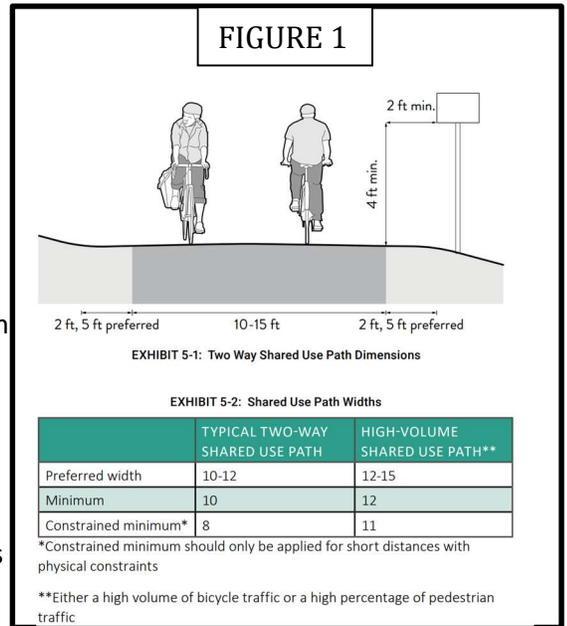
For trail located outside of Rice Creek Commons, the City of Arden Hills standards for multi-use trails call for a minimum 8-foot width. The applicable language within the Arden Hills Public Works Design Manual (Draft), Street Section, Item 19 states:

“When trails are proposed, trails shall be constructed of bituminous with a minimum width of 8-feet and minimum thickness of 3-inches with an aggregate base thickness of 6-inches.”

MnDOT Guidance and PROWAG Requirements:

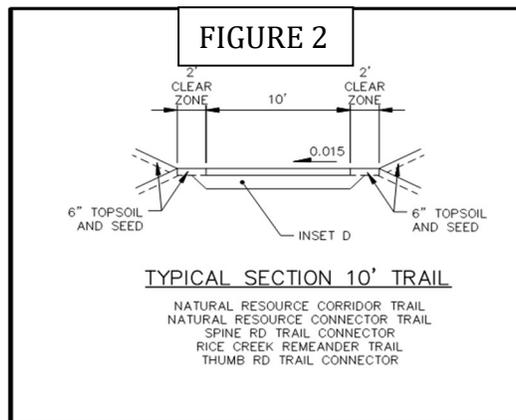
MnDOT’s Bikeway Facility Design Guide provides recommended multi-use trail widths for various purposes. The manual provides a minimum of 10ft in unconstrained areas for a Two-Way multi-use trail. This width allows for a bicyclist traveling single file to pass someone coming from the opposite direction without conflict, or for two bicyclists to ride comfortably side-by-side, effectively a “two-lane” path (per MnDOT Bikeway Facility Design Guide – see 5-4 for figure 1).

Additionally, A **10ft wide** shared use path meets the minimum width requirements for Pedestrian Access Route (per ADA and Public Right-of-Way Accessibility Guidelines (PROWAG)) governed under the Americans with Disabilities Act (ADA).



Past RCC (fka TCAAP) Precedence:

The past design plans, dated 2019, had also planned for 10ft wide multi-use trails throughout the project including all Natural Resource Corridor Trails, Spine Road Trails, and the Re-meander Trail under the future Bridge. See figure 2 for details.



Item #2: Street Lighting along Rice Creek Boulevard

Introduction Statement: The TRC’s guidance, as currently written, would either direct the design to include no lighting along Rice Creek Boulevard or alternatively, if lighting is provided, it is required to be at a spacing (40-ft) that is much closer than is typical or desirable for this type of road. The requested amendment would enable appropriate spacing of lighting.

Current Applicable Code:

TRC Section 10.5, Street Lighting

“Street lighting should be provided on Spine, Town, Collector and Collector with Trail roads. Refer to Arden Hills Public Works Design Manual for lighting palette. The following standards should apply for pedestrian scale lighting:

- (a) Vehicular lighting shall be a maximum of thirty feet (30’) in height and pedestrian-oriented lighting a maximum of eighteen feet (18’) in height.*
- (b) Streetlights shall be placed at an average of forty feet (40’) on center, approximately three (3) feet behind the back of curb, aligned with street trees, and where street trees are planted.”*

Amended Applicable Code (Proposed):

TRC Section 10.5, Street Lighting

“Street lighting should be provided on Spine, Town, Collector and Collector with Trail roads. Refer to Arden Hills Public Works Design Manual for lighting palette. The following standards should apply for pedestrian scale lighting:

- (a) Vehicular lighting shall be a maximum of thirty feet (30’) in height and pedestrian-oriented lighting a maximum of eighteen feet (18’) in height.*
- (b) All lighting shall be placed at spacing adequate to meet Recommended Footcandle Levels as established in Section 4.1.1.3 of the MnDOT Roadway Lighting Design Manual.*
- (c) Vehicular lights shall be placed approximately three (3) feet behind the back of curb.*
- (d) Pedestrian-oriented lights shall be placed approximately two (2) feet away from sidewalks or trails within proposed green space and outside of required clear zones”*

Practical Difficulty of meeting TRC (Current Applicable Code):

No Lighting along Rice Creek Boulevard:

As written, the applicable code appears to allow for an approach where no lighting would be required along Rice Creek Boulevard. However, this approach would not be consistent with best safety practices nor City/County design precedence at the proposed roundabout intersections. The need to provide intersection lighting does not allow for compliance with the TRC through providing no lighting along Rice Creek Boulevard.

Providing Lighting (per TRC) along Rice Creek Boulevard:

The applicable code (as provided above) would require streetlights to be installed at an average of 40-foot on center for the length of the Rice Creek Boulevard Corridor. Application of this standard would require the installation of approximately 200 streetlights, estimated to cost \$2,000,000. A more context appropriate approach to streetlight spacing (~200-ft) would reduce the number of streetlights to approximately 40 lights, or \$400,000 in estimated construction costs.

*Additional Construction Cost: \$1,600,000**

*Table 3.3 of the TRC does allow for adjustments to the applicable code of up to 10%. Maximizing the permitted adjustment would reduce the required streetlights to 180 lights. In this scenario the additional construction cost is reduced to **\$1,400,000**.

In addition to the initial capital investment of installing the streetlights, long-term maintenance and operation costs should also be considered. Maintenance and operations costs would be proportionally (per light) more expensive for the more intensive lighting approach.

Design Standards/Justification for Proposed Code Amendment:

Roundabout Lighting: MnDOT and FHWA design guidance recommends that all roundabouts be lit to provide adequate visibility for all modes of transportation during non-daylight hours. This guidance is consistent with past precedence for roundabouts constructed by Ramsey County and the City of Arden Hills.

Vehicular Lighting: Street lighting is designed based on various physical factors of the lighting pole, trajectory, and luminaire intensity. The City of Arden Hills Public Works Design Manual (Draft) specifies the light type and design parameter for the lighting to be installed along Rice Creek Boulevard. Application of MnDOT photometrics design guidance, the specified lighting units would result in recommended spacing of approximately 200-ft. Detailed design of the lighting system would prioritize light locations at intersections and pedestrian crossings which reduce the space between lights in those locations. The following information is attached to this memo in the appendices providing further detail of the vehicular lighting design:

- Appendix A: Photometric layouts for 40-ft vehicular lighting per current TRC code.
- Appendix B: Photometric layouts for the proposed vehicular lighting meeting standards established in the MnDOT Roadway Lighting Design Manual.

The table below summarizes the design criteria of the MnDOT Roadway Lighting Design Manual and the performance of each of the two options against the standards.

Design Alternative	Minimum Avg Illuminance (Fc)	Design Avg Illuminance (Fc)	Minimum Uniformity (Avg/Min)	Design Uniformity (Avg/Min)
40-ft Light Spacing (Current TRC)	<i>1.0</i>	7.32-8.11	<i>4.0</i>	1.29-1.44
Proposed Light Spacing (MnDOT)	<i>1.0</i>	1.05-1.20	<i>4.0</i>	3.0-3.5

Local Area Precedence (Vehicular Lighting): The following corridors/intersections have been constructed with streetlighting within the project vicinity. Design parameters for those systems are provided for reference.

Rice Creek Parkway (County Road I to County Road J)

Light Type Comparison: Shorter, more decorative luminaire

Light Spacing: Approximately 200 feet

Speed Limit: 30 mph

Snelling Avenue/County Road E Roundabout

Light Type Comparison: Similar height and luminaire

Light Spacing: Lights at all intersection quadrants, 140-200 feet exiting RAB

Speed Limit: 25-35 mph, 45 mph north on Snelling Ave

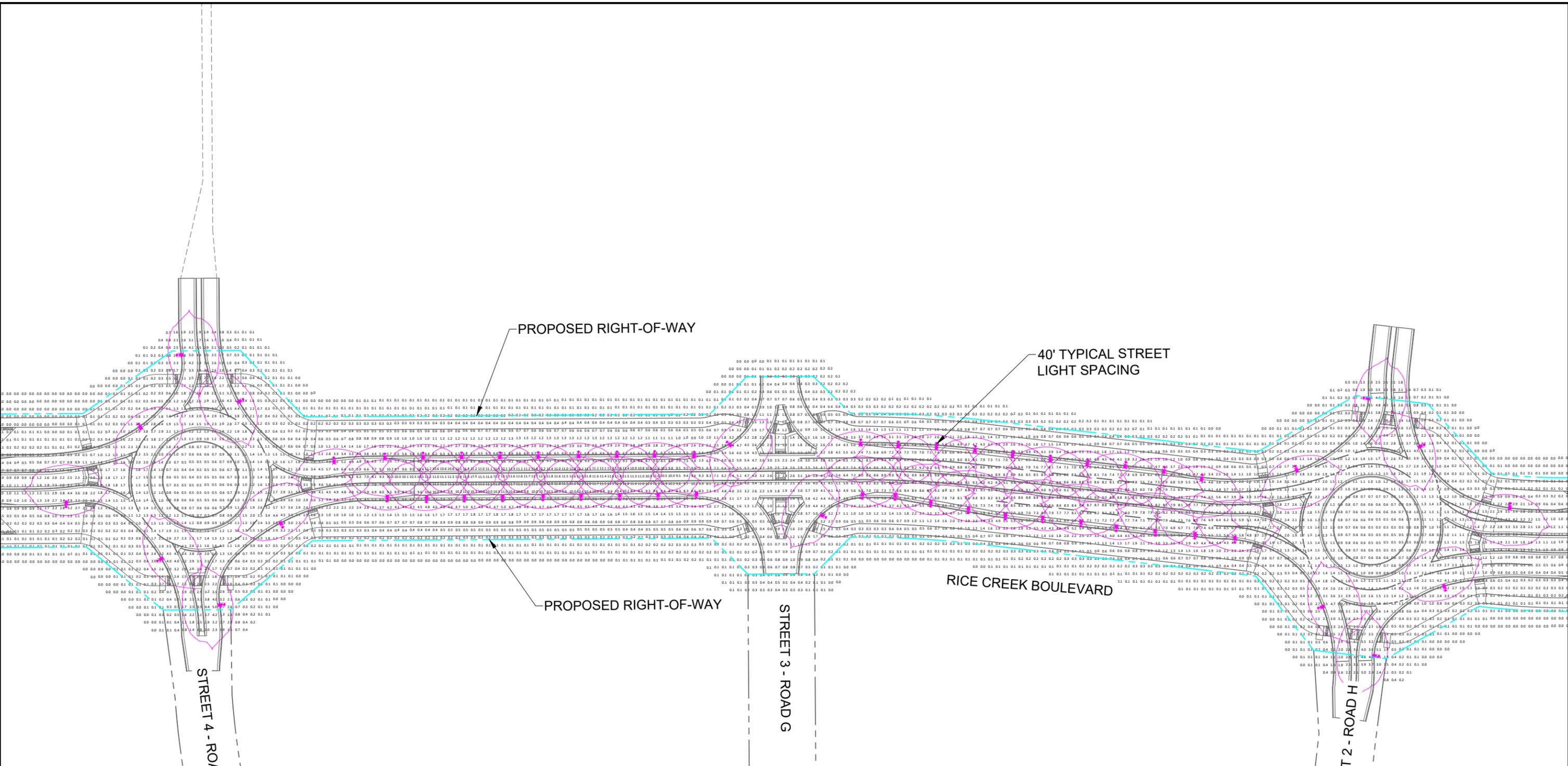
Pedestrian Orientated Lighting: The current TRC does not specifically require the installation of pedestrian oriented lighting but does establish design and spacing requirements if it is installed. The proposed TRC amendment language references the MnDOT Roadway Lighting Design Manual which also provides guidance for pedestrian-oriented lighting. The resultant spacing of the pedestrian-oriented lights would be approximately 50-ft. Appendix C is attached to this memo providing photometric layouts for the potential pedestrian-oriented lights meeting MnDOT Roadway Lighting Design Manual standards. The table below summarizes the design criteria and performance of the lighting shown in Appendix C.

	Minimum Avg Illuminance (Fc)	Design Avg Illuminance (Fc)	Minimum Uniformity (Avg/Min)	Design Uniformity (Avg/Min)
Pedestrian Light Spacing (MnDOT)	<i>2.0</i>	2.15-2.16	<i>3.0</i>	1.43-1.44

APPENDIX A

Lighting Photometric Layouts

40-ft Vehicular Light Spacing (Per Current TRC)



40-FT VEHICULAR LIGHT SPACING COMMERCIAL ZONE

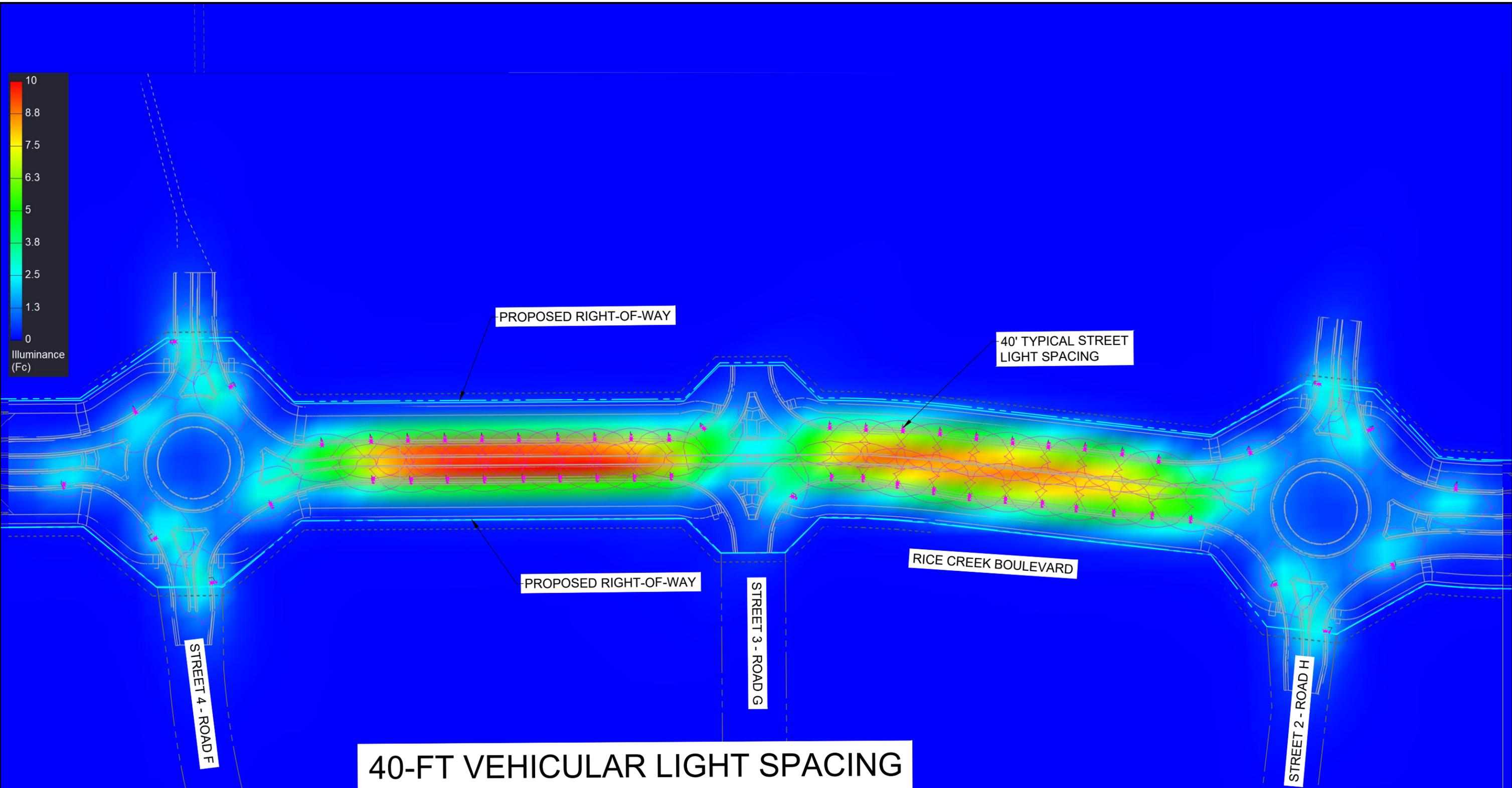
LUMINAIRE SCHEDULE - 40' SPACING (PER TRC)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
★	392	14000	150	58800
★	0	4000	30	0

FOOT CANDLE LEVELS - 40 SPACING (PER TRC)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	7.32	4.0	1.44
TRAIL	2.0	0.55	3.0	1.83

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				N/A

CITY PUBLIC WORKS LIGHTING STYLES

<p>STREET LIGHTS:</p> <p>HEIGHT = 30 FT POLE STYLE = A - ALUMINUM POLE COLOR = BLACK</p> <p>LED LUMINAIRE STYLE = COBRA LUMINAIRE COLOR = BLACK</p> <p>PLAN SYMBOL: ★</p>	<p>PEDESTRIAN LIGHTS:</p> <p>HEIGHT = 18 FT POLE STYLE = B - COLUMBUS POLE COLOR = BLACK</p> <p>LED LUMINAIRE STYLE = LANTERN LUMINAIRE COLOR = BLACK</p> <p>PLAN SYMBOL: ★</p>
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40-FT VEHICULAR LIGHT SPACING COMMERCIAL ZONE

LUMINAIRE SCHEDULE - 40' SPACING (PER TRC)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	392	14000	150	58800
	0	4000	30	0

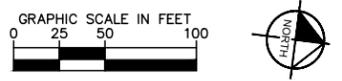
FOOT CANDLE LEVELS - 40' SPACING (PER TRC)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	7.32	4.0	1.44
TRAIL	2.0	0.55	3.0	1.83

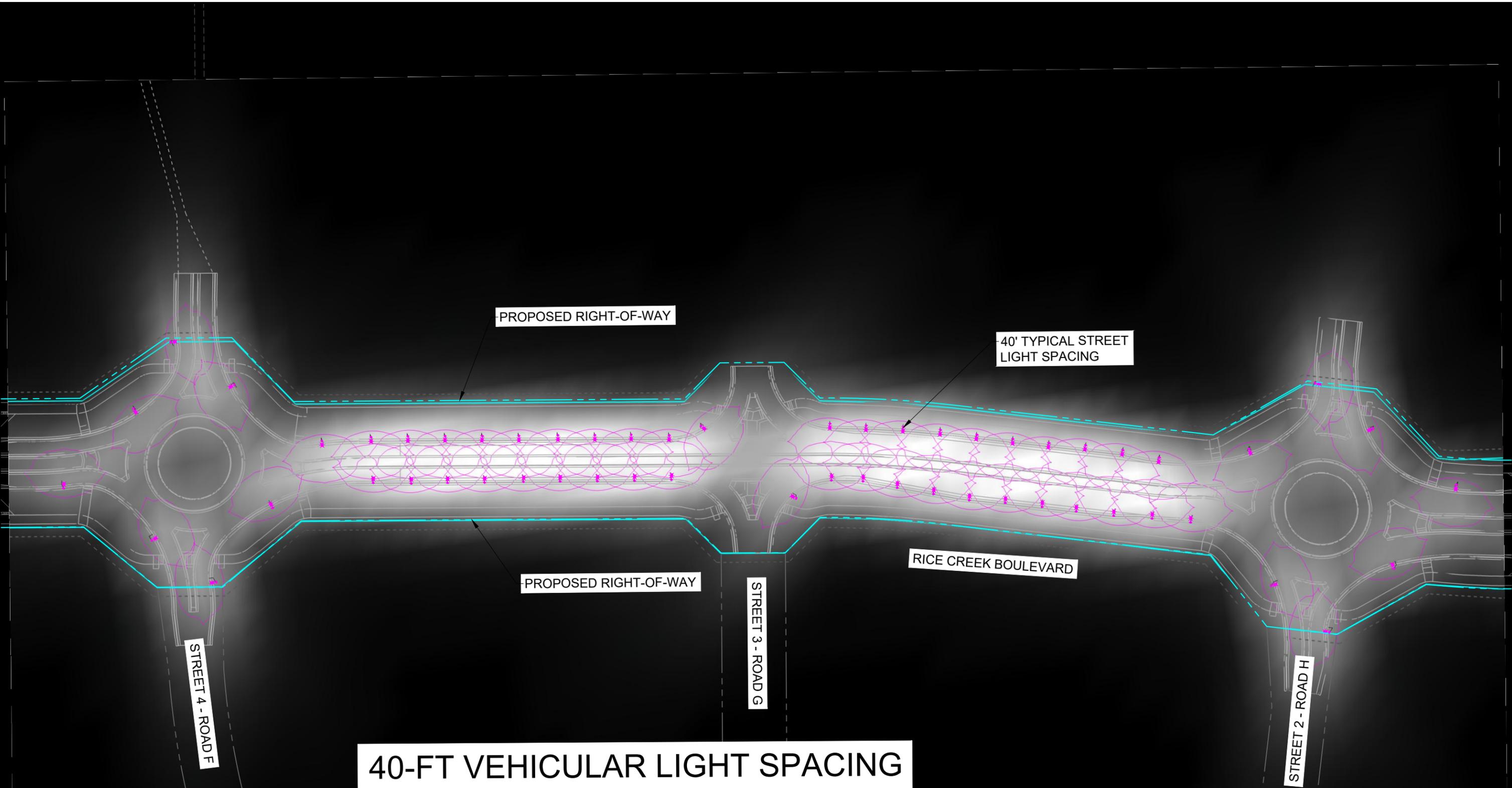
MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				N/A

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:
 HEIGHT = 30 FT
 POLE STYLE = A - ALUMINUM
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = COBRA
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:

PEDESTRIAN LIGHTS:
 HEIGHT = 18 FT
 POLE STYLE = B - COLUMBUS
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = LANTERN
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:





PROPOSED RIGHT-OF-WAY

40' TYPICAL STREET LIGHT SPACING

PROPOSED RIGHT-OF-WAY

RICE CREEK BOULEVARD

STREET 4 - ROAD F

STREET 3 - ROAD G

STREET 2 - ROAD H

40-FT VEHICULAR LIGHT SPACING COMMERCIAL ZONE

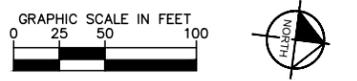
LUMINAIRE SCHEDULE - 40' SPACING (PER TRC)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	392	14000	150	58800
	0	4000	30	0

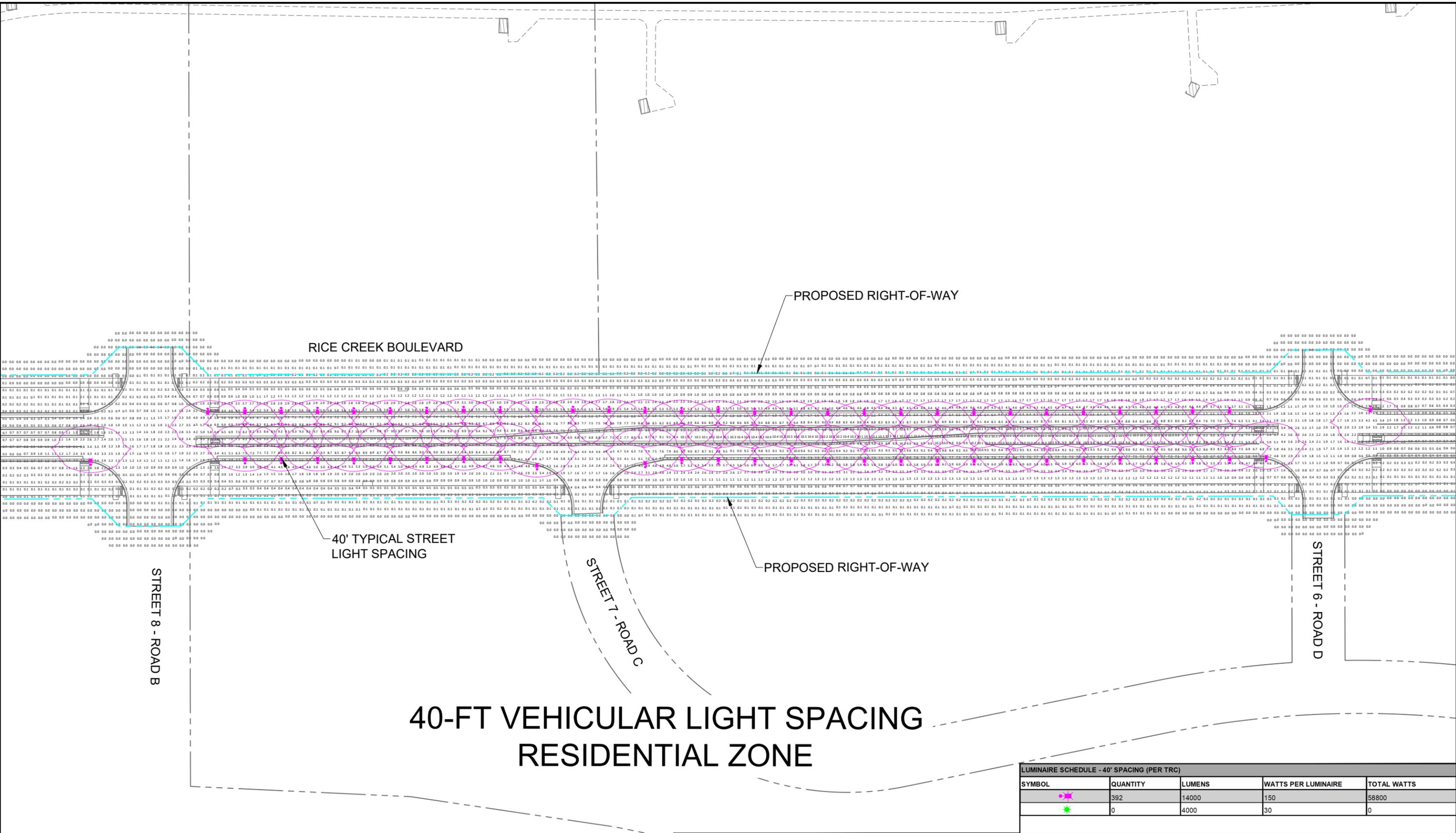
FOOT CANDLE LEVELS - 40' SPACING (PER TRC)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	7.32	4.0	1.44
TRAIL	2.0	0.55	3.0	1.83

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				N/A

CITY PUBLIC WORKS LIGHTING STYLES

<p>STREET LIGHTS:</p> <p>HEIGHT = 30 FT POLE STYLE = A - ALUMINUM POLE COLOR = BLACK</p> <p>LED LUMINAIRE STYLE = COBRA LUMINAIRE COLOR = BLACK</p> <p>PLAN SYMBOL: </p>	<p>PEDESTRIAN LIGHTS:</p> <p>HEIGHT = 18 FT POLE STYLE = B - COLUMBUS POLE COLOR = BLACK</p> <p>LED LUMINAIRE STYLE = LANTERN LUMINAIRE COLOR = BLACK</p> <p>PLAN SYMBOL: </p>
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RICE CREEK BOULEVARD

PROPOSED RIGHT-OF-WAY

40' TYPICAL STREET LIGHT SPACING

PROPOSED RIGHT-OF-WAY

STREET 8 - ROAD B

STREET 7 - ROAD C

STREET 6 - ROAD D

40-FT VEHICULAR LIGHT SPACING RESIDENTIAL ZONE

LUMINAIRE SCHEDULE - 40' SPACING (PER TRC)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	392	14000	150	58800
	0	4000	30	0

FOOT CANDLE LEVELS - 40 SPACING (PER TRC)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	8.11	4.0	1.29
TRAIL	2.0	0.48	3.0	1.60

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				0.2

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:

HEIGHT = 30 FT
POLE STYLE = A - ALUMINUM
POLE COLOR = BLACK

LED LUMINAIRE STYLE = COBRA
LUMINAIRE COLOR = BLACK

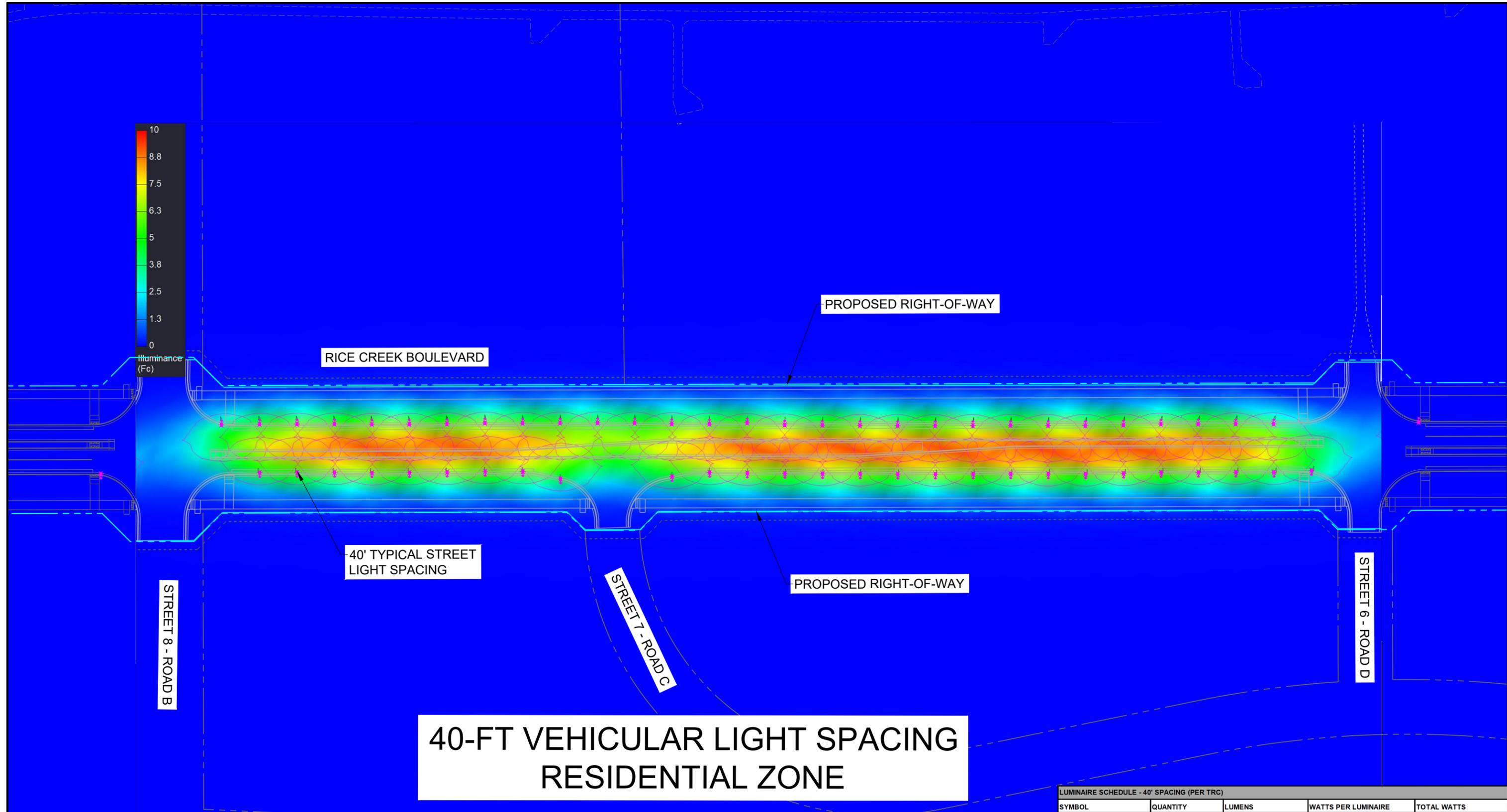
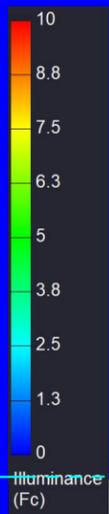
PLAN SYMBOL:

PEDESTRIAN LIGHTS:

HEIGHT = 18 FT
POLE STYLE = B - COLUMBUS
POLE COLOR = BLACK

LED LUMINAIRE STYLE = LANTERN
LUMINAIRE COLOR = BLACK

PLAN SYMBOL:



40-FT VEHICULAR LIGHT SPACING RESIDENTIAL ZONE

LUMINAIRE SCHEDULE - 40' SPACING (PER TRC)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
(Cobra Symbol)	392	14000	150	58800
(Lantern Symbol)	0	4000	30	0

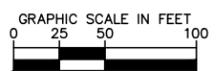
FOOT CANDLE LEVELS - 40' SPACING (PER TRC)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	8.11	4.0	1.29
TRAIL	2.0	0.48	3.0	1.60

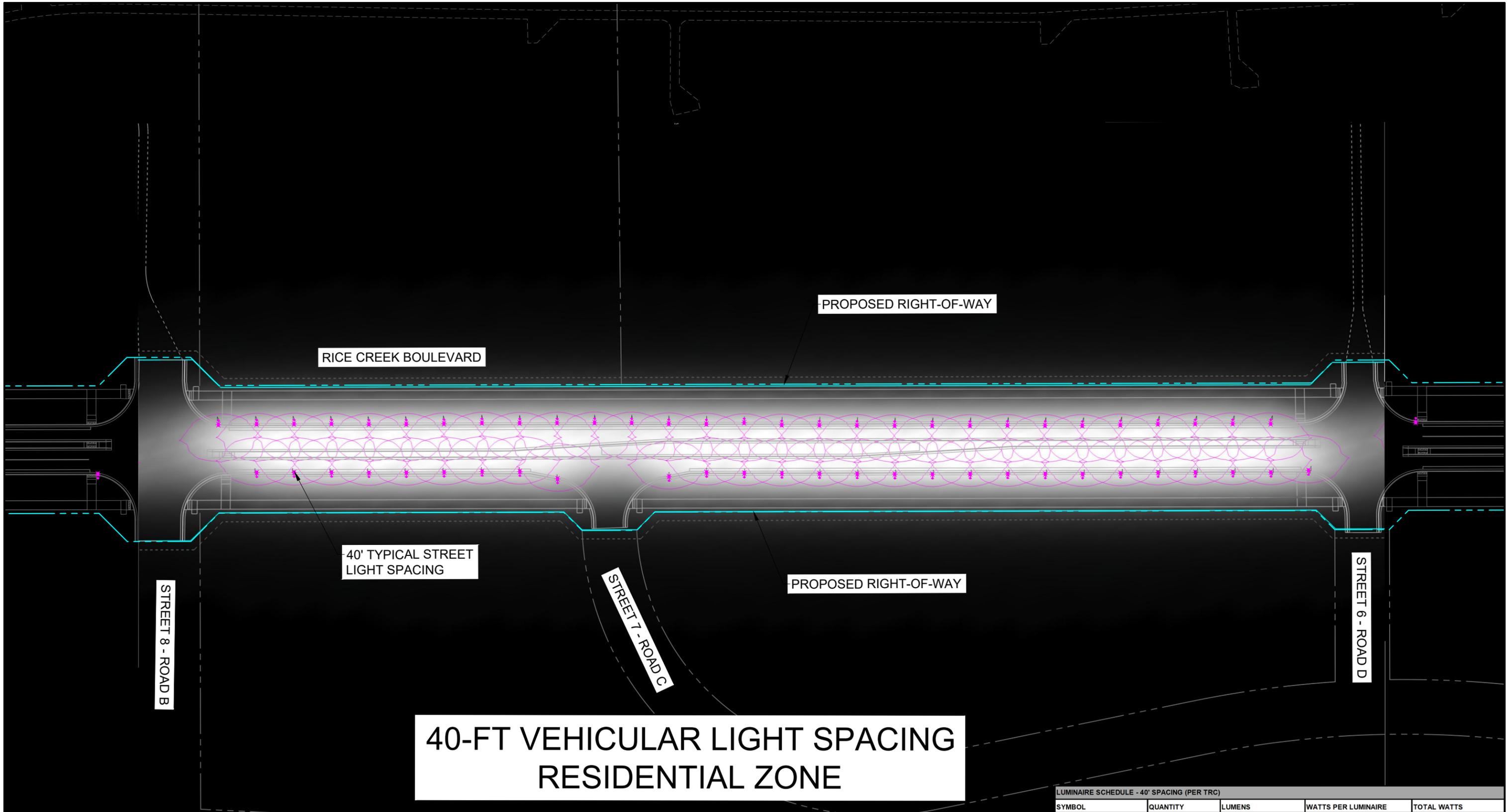
MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	0.2

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:
 HEIGHT = 30 FT
 POLE STYLE = A - ALUMINUM
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = COBRA
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:

PEDESTRIAN LIGHTS:
 HEIGHT = 18 FT
 POLE STYLE = B - COLUMBUS
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = LANTERN
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:





40-FT VEHICULAR LIGHT SPACING RESIDENTIAL ZONE

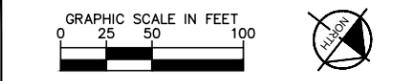
LUMINAIRE SCHEDULE - 40' SPACING (PER TRC)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	392	14000	150	58800
	0	4000	30	0

FOOT CANDLE LEVELS - 40' SPACING (PER TRC)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	8.11	4.0	1.29
TRAIL	2.0	0.48	3.0	1.60

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	0.2

CITY PUBLIC WORKS LIGHTING STYLES

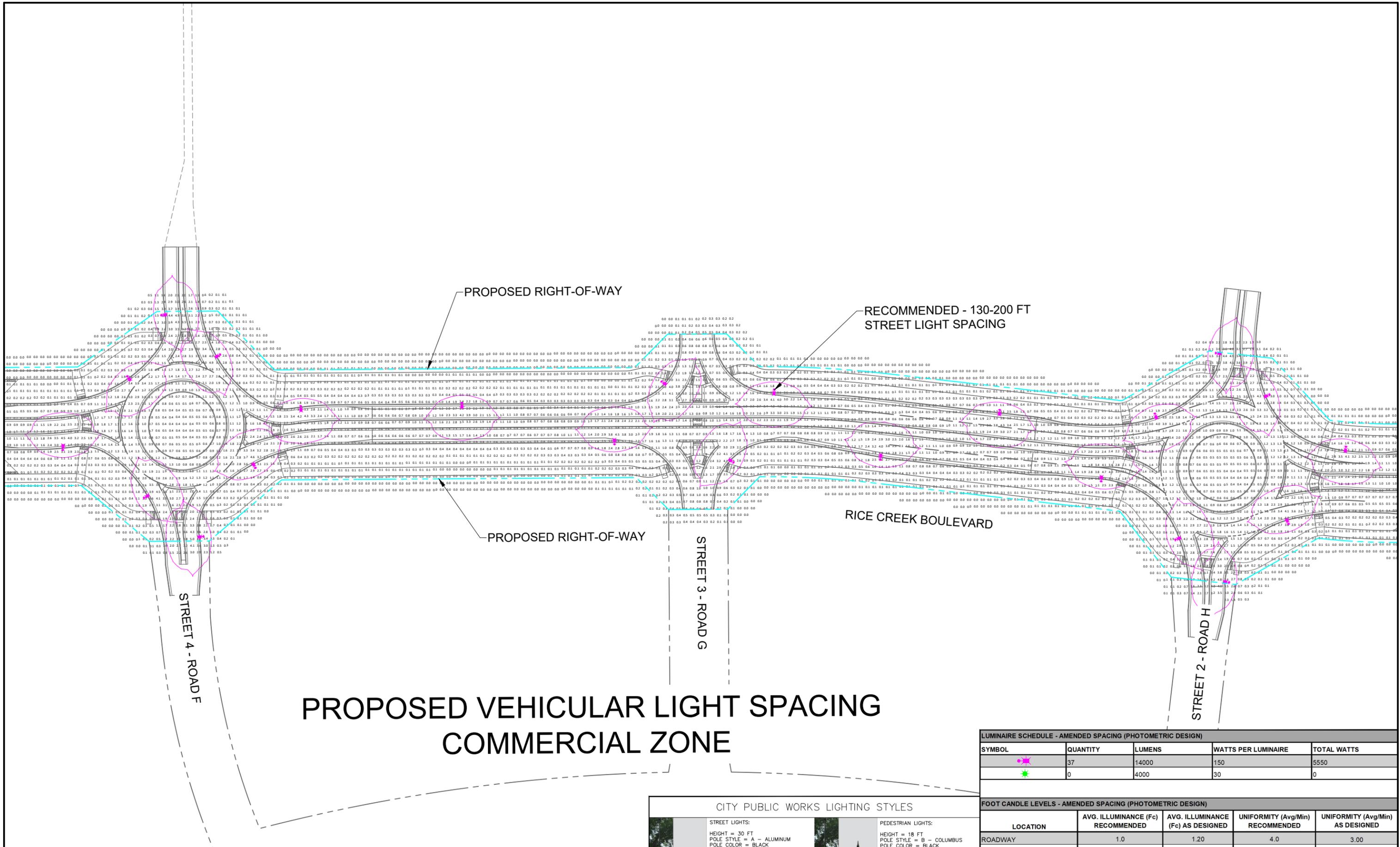
<p>STREET LIGHTS: HEIGHT = 30 FT POLE STYLE = A - ALUMINUM POLE COLOR = BLACK LED LUMINAIRE STYLE = COBRA LUMINAIRE COLOR = BLACK PLAN SYMBOL: </p>	<p>PEDESTRIAN LIGHTS: HEIGHT = 18 FT POLE STYLE = B - COLUMBUS POLE COLOR = BLACK LED LUMINAIRE STYLE = LANTERN LUMINAIRE COLOR = BLACK PLAN SYMBOL: </p>
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APPENDIX B

Lighting Photometric Layouts

Proposed Vehicular Light Spacing (Per MnDOT Roadway Lighting Design Manual)



PROPOSED VEHICULAR LIGHT SPACING COMMERCIAL ZONE

LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
✳	37	14000	150	5550
✳	0	4000	30	0

FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.20	4.0	3.00
TRAIL	2.0	0.06	3.0	N/A

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	N/A

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:

HEIGHT = 30 FT
POLE STYLE = A - ALUMINUM
POLE COLOR = BLACK

LED LUMINAIRE STYLE = COBRA
LUMINAIRE COLOR = BLACK

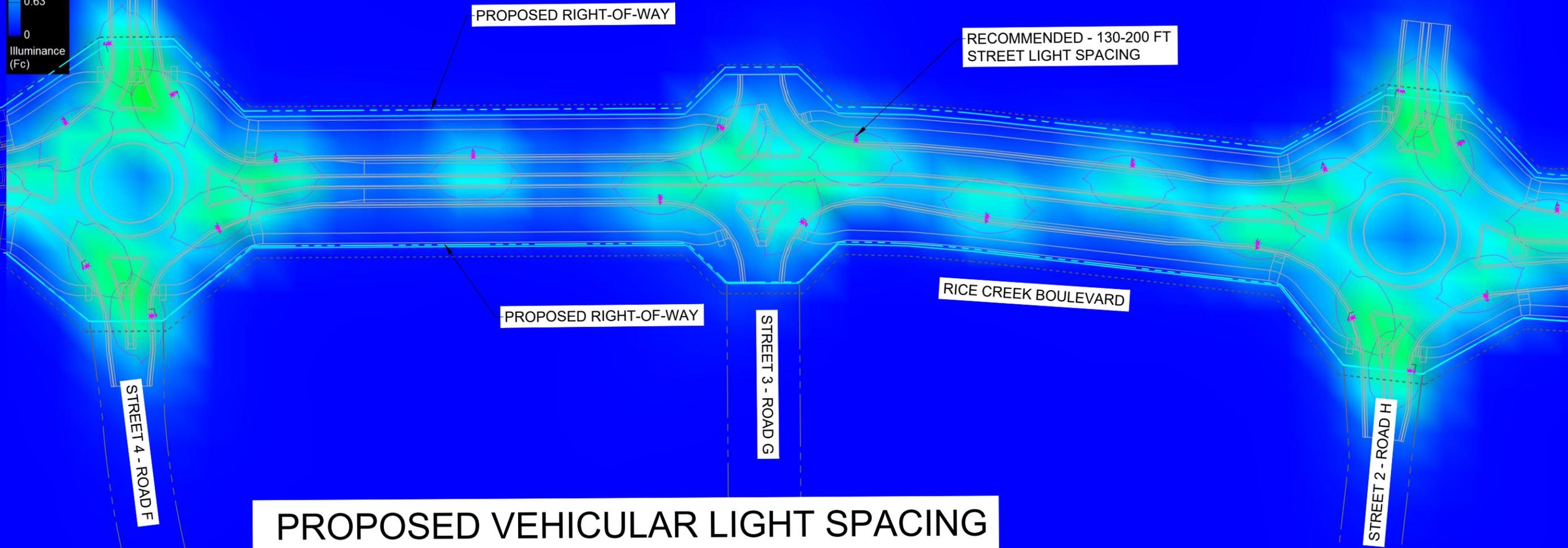
PLAN SYMBOL: ✳

PEDESTRIAN LIGHTS:

HEIGHT = 18 FT
POLE STYLE = B - COLUMBUS
POLE COLOR = BLACK

LED LUMINAIRE STYLE = LANTERN
LUMINAIRE COLOR = BLACK

PLAN SYMBOL: ✳



PROPOSED VEHICULAR LIGHT SPACING COMMERCIAL ZONE

LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
	0	4000	30	0

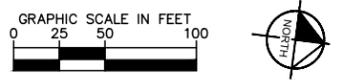
FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.20	4.0	3.00
TRAIL	2.0	0.06	3.0	N/A

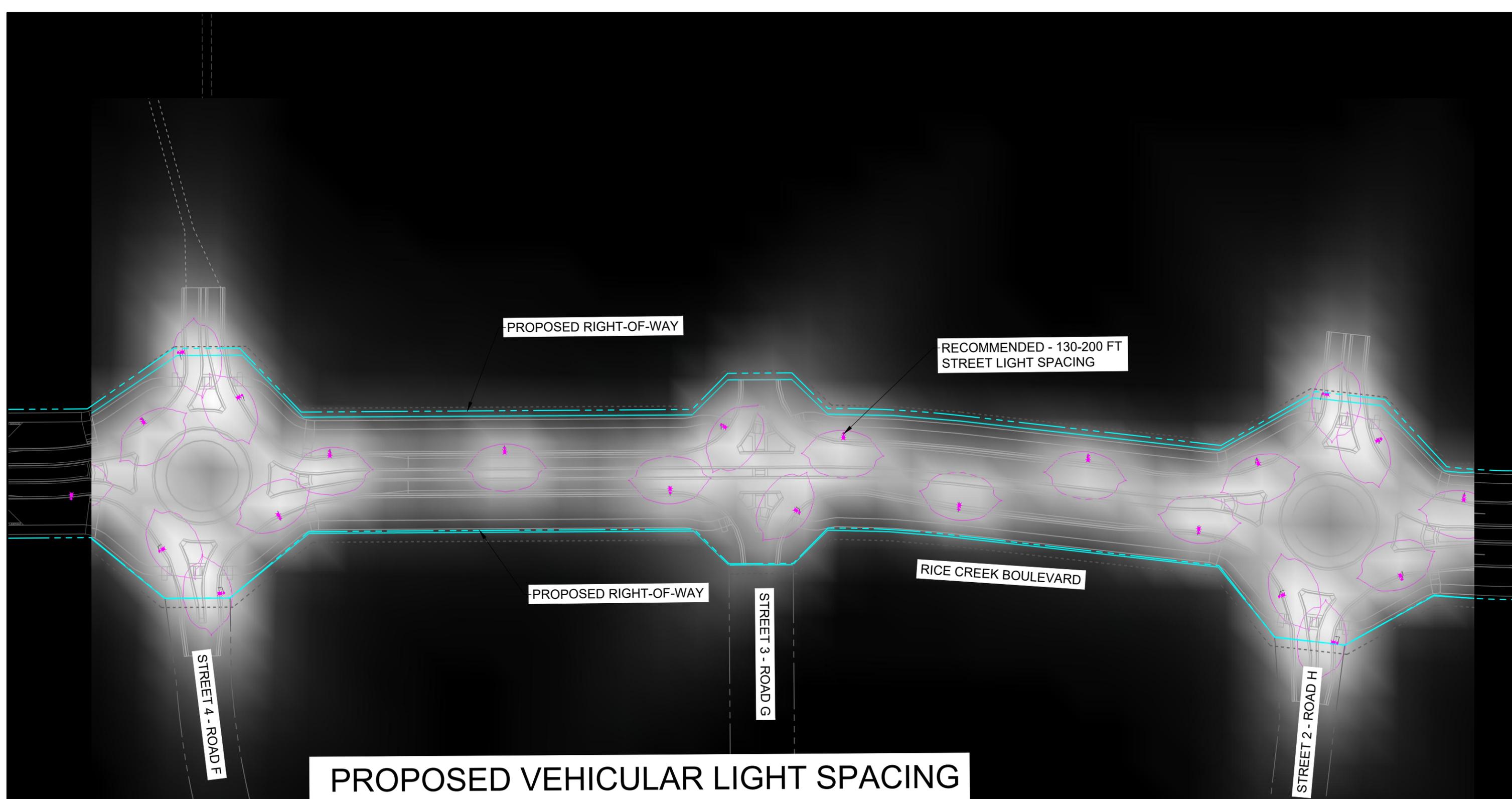
MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	N/A

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:
 HEIGHT = 30 FT
 POLE STYLE = A - ALUMINUM
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = COBRA
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:

PEDESTRIAN LIGHTS:
 HEIGHT = 18 FT
 POLE STYLE = B - COLUMBUS
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = LANTERN
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:





PROPOSED VEHICULAR LIGHT SPACING COMMERCIAL ZONE

LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
	0	4000	30	0

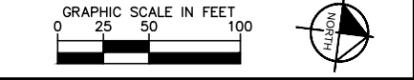
FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.20	4.0	3.00
TRAIL	2.0	0.06	3.0	N/A

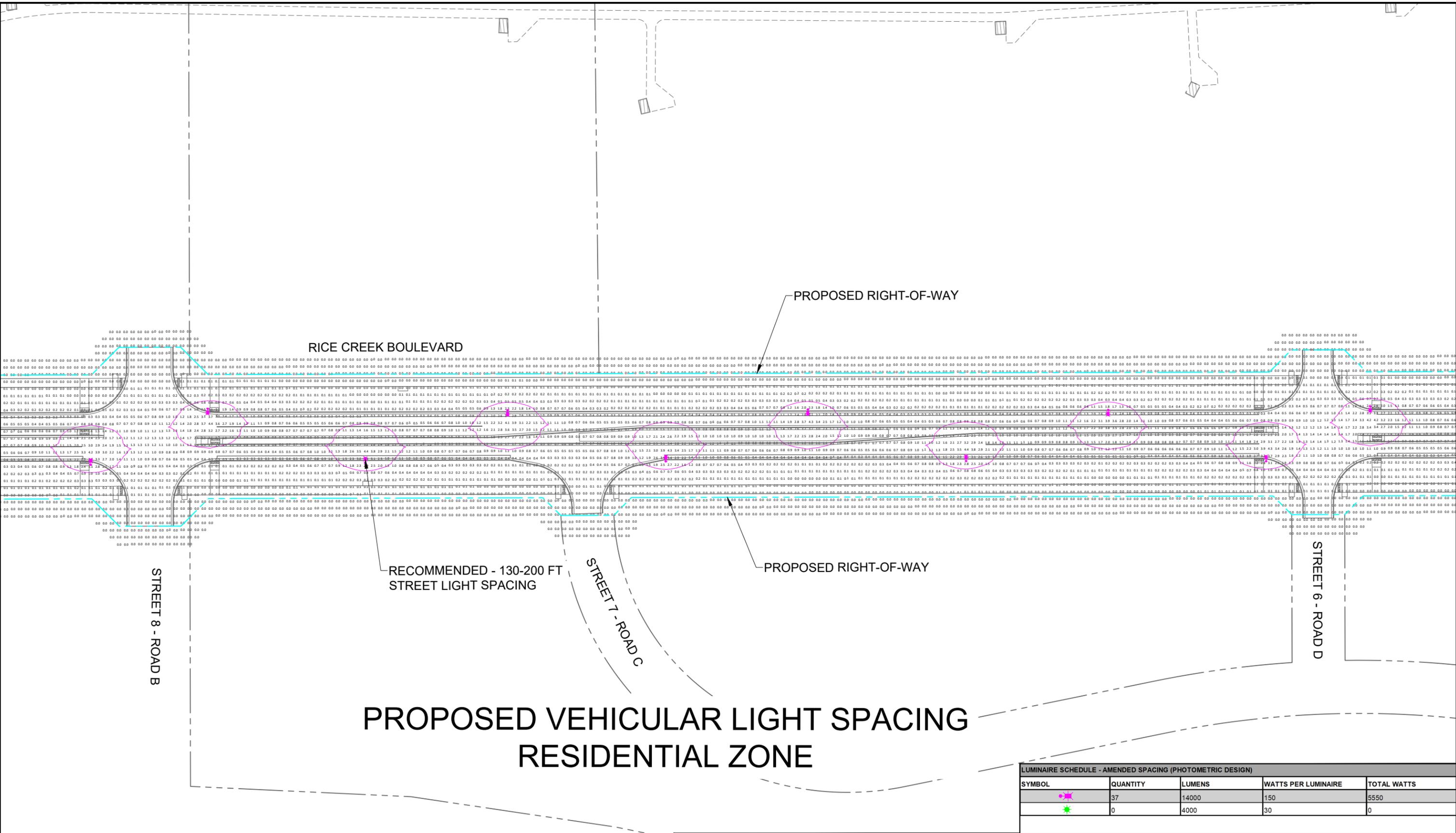
MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	N/A

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:
 HEIGHT = 30 FT
 POLE STYLE = A - ALUMINUM
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = COBRA
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:

PEDESTRIAN LIGHTS:
 HEIGHT = 18 FT
 POLE STYLE = B - COLUMBUS
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = LANTERN
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:





RICE CREEK BOULEVARD

PROPOSED RIGHT-OF-WAY

RECOMMENDED - 130-200 FT STREET LIGHT SPACING

PROPOSED RIGHT-OF-WAY

STREET 8 - ROAD B

STREET 7 - ROAD C

STREET 6 - ROAD D

PROPOSED VEHICULAR LIGHT SPACING RESIDENTIAL ZONE

LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
	0	4000	30	0

FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.05	4.0	3.50
TRAIL	2.0	0.02	3.0	N/A

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				0.0

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:

HEIGHT = 30 FT
POLE STYLE = A - ALUMINUM
POLE COLOR = BLACK

LED LUMINAIRE STYLE = COBRA
LUMINAIRE COLOR = BLACK

PLAN SYMBOL:

PEDESTRIAN LIGHTS:

HEIGHT = 18 FT
POLE STYLE = B - COLUMBUS
POLE COLOR = BLACK

LED LUMINAIRE STYLE = LANTERN
LUMINAIRE COLOR = BLACK

PLAN SYMBOL:



RICE CREEK BOULEVARD

PROPOSED RIGHT-OF-WAY

RECOMMENDED - 130-200 FT STREET LIGHT SPACING

PROPOSED RIGHT-OF-WAY

STREET 8 - ROAD B

STREET 7 - ROAD C

STREET 6 - ROAD D

PROPOSED VEHICULAR LIGHT SPACING RESIDENTIAL ZONE

LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
	0	4000	30	0

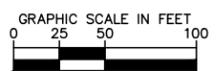
FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.05	4.0	3.50
TRAIL	2.0	0.02	3.0	N/A

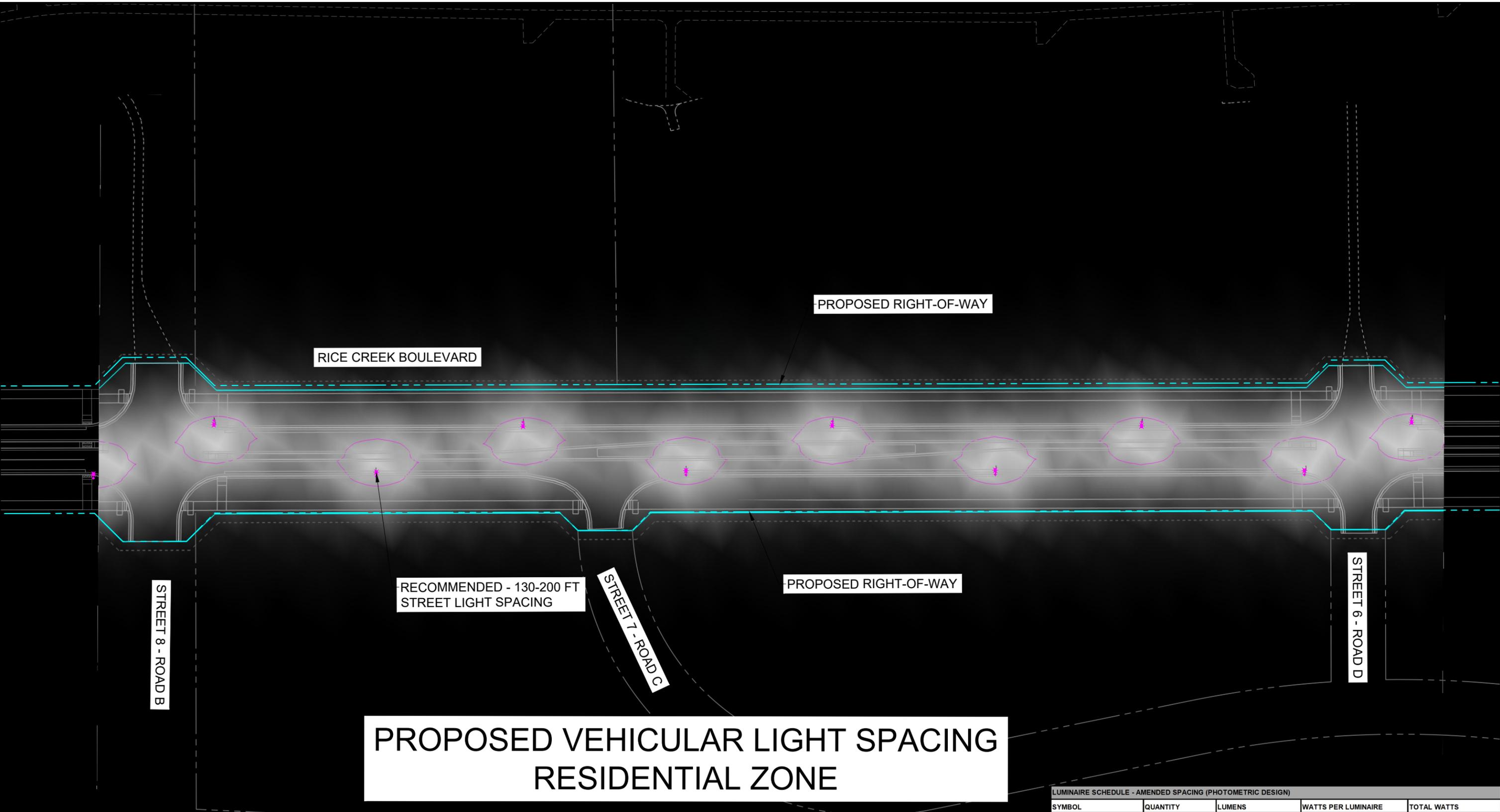
MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	0.0

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:
HEIGHT = 30 FT
POLE STYLE = A - ALUMINUM
POLE COLOR = BLACK
LED LUMINAIRE STYLE = COBRA
LUMINAIRE COLOR = BLACK
PLAN SYMBOL:

PEDESTRIAN LIGHTS:
HEIGHT = 18 FT
POLE STYLE = B - COLUMBUS
POLE COLOR = BLACK
LED LUMINAIRE STYLE = LANTERN
LUMINAIRE COLOR = BLACK
PLAN SYMBOL:





**PROPOSED VEHICULAR LIGHT SPACING
RESIDENTIAL ZONE**

RECOMMENDED - 130-200 FT
STREET LIGHT SPACING

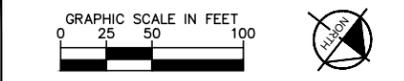
LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN)				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
	0	4000	30	0

FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.05	4.0	3.50
TRAIL	2.0	0.02	3.0	N/A

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	0.0

CITY PUBLIC WORKS LIGHTING STYLES

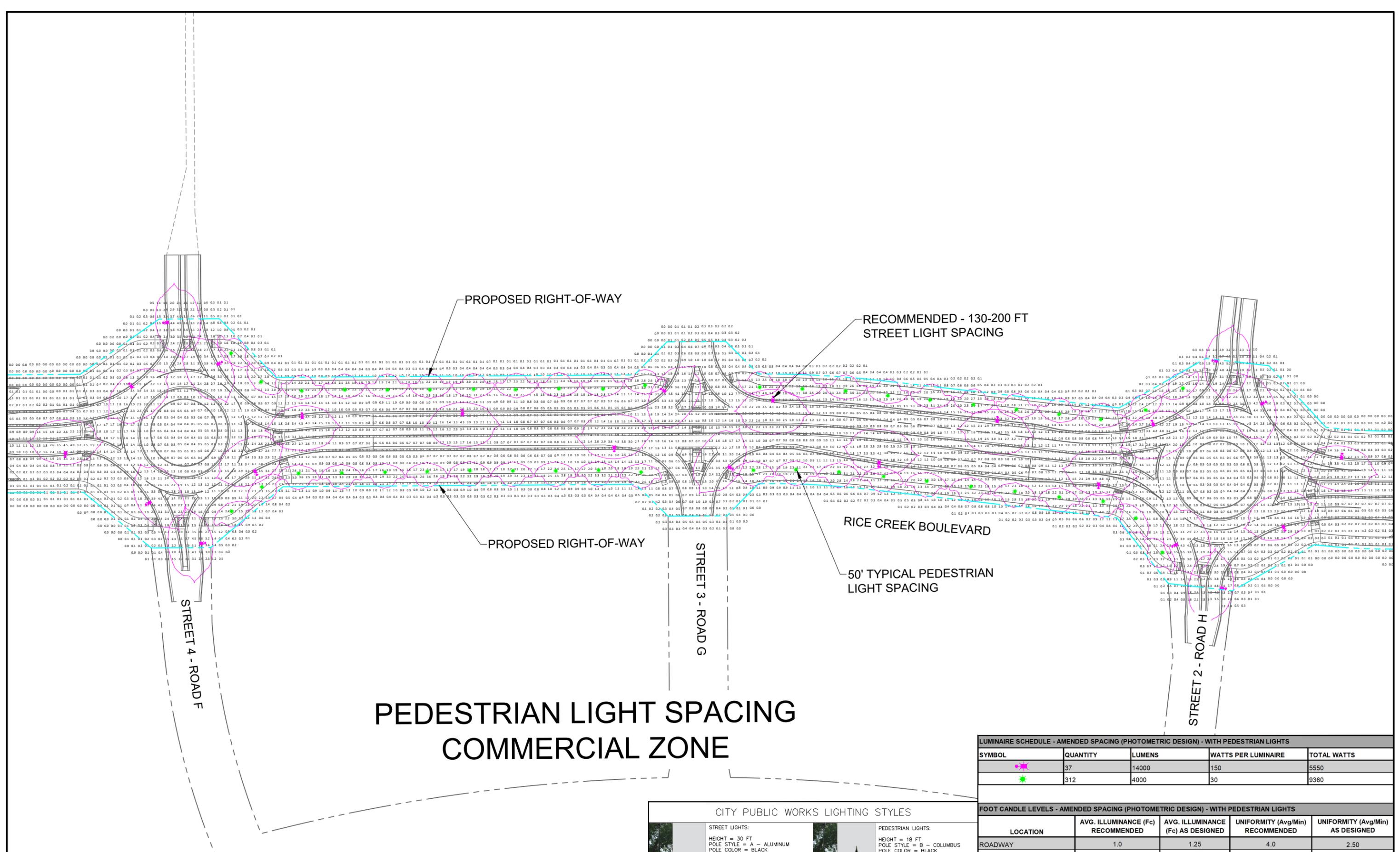
<p>STREET LIGHTS: HEIGHT = 30 FT POLE STYLE = A - ALUMINUM POLE COLOR = BLACK LED LUMINAIRE STYLE = COBRA LUMINAIRE COLOR = BLACK PLAN SYMBOL: </p>	<p>PEDESTRIAN LIGHTS: HEIGHT = 18 FT POLE STYLE = B - COLUMBUS POLE COLOR = BLACK LED LUMINAIRE STYLE = LANTERN LUMINAIRE COLOR = BLACK PLAN SYMBOL: </p>
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APPENDIX C

Lighting Photometric Layouts

Potential Pedestrian-Orientated Light Spacing (Per MnDOT Roadway Lighting Design Manual)



PEDESTRIAN LIGHT SPACING COMMERCIAL ZONE

LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
	312	4000	30	9360

FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.25	4.0	2.50
TRAIL	2.0	2.15	3.0	1.43

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				N/A

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:

HEIGHT = 30 FT
POLE STYLE = A - ALUMINUM
POLE COLOR = BLACK

LED LUMINAIRE STYLE = COBRA
LUMINAIRE COLOR = BLACK

PLAN SYMBOL:

PEDESTRIAN LIGHTS:

HEIGHT = 18 FT
POLE STYLE = B - COLUMBUS
POLE COLOR = BLACK

LED LUMINAIRE STYLE = LANTERN
LUMINAIRE COLOR = BLACK

PLAN SYMBOL:



PEDESTRIAN LIGHT SPACING COMMERCIAL ZONE

PROPOSED RIGHT-OF-WAY

RECOMMENDED - 130-200 FT
STREET LIGHT SPACING

PROPOSED RIGHT-OF-WAY

RICE CREEK BOULEVARD

50' TYPICAL PEDESTRIAN
LIGHT SPACING

STREET 4 - ROAD F

STREET 3 - ROAD G

STREET 2 - ROAD H

LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
	312	4000	30	9360

FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.25	4.0	2.50
TRAIL	2.0	2.15	3.0	1.43

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	N/A

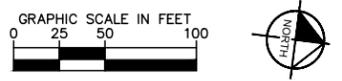
CITY PUBLIC WORKS LIGHTING STYLES

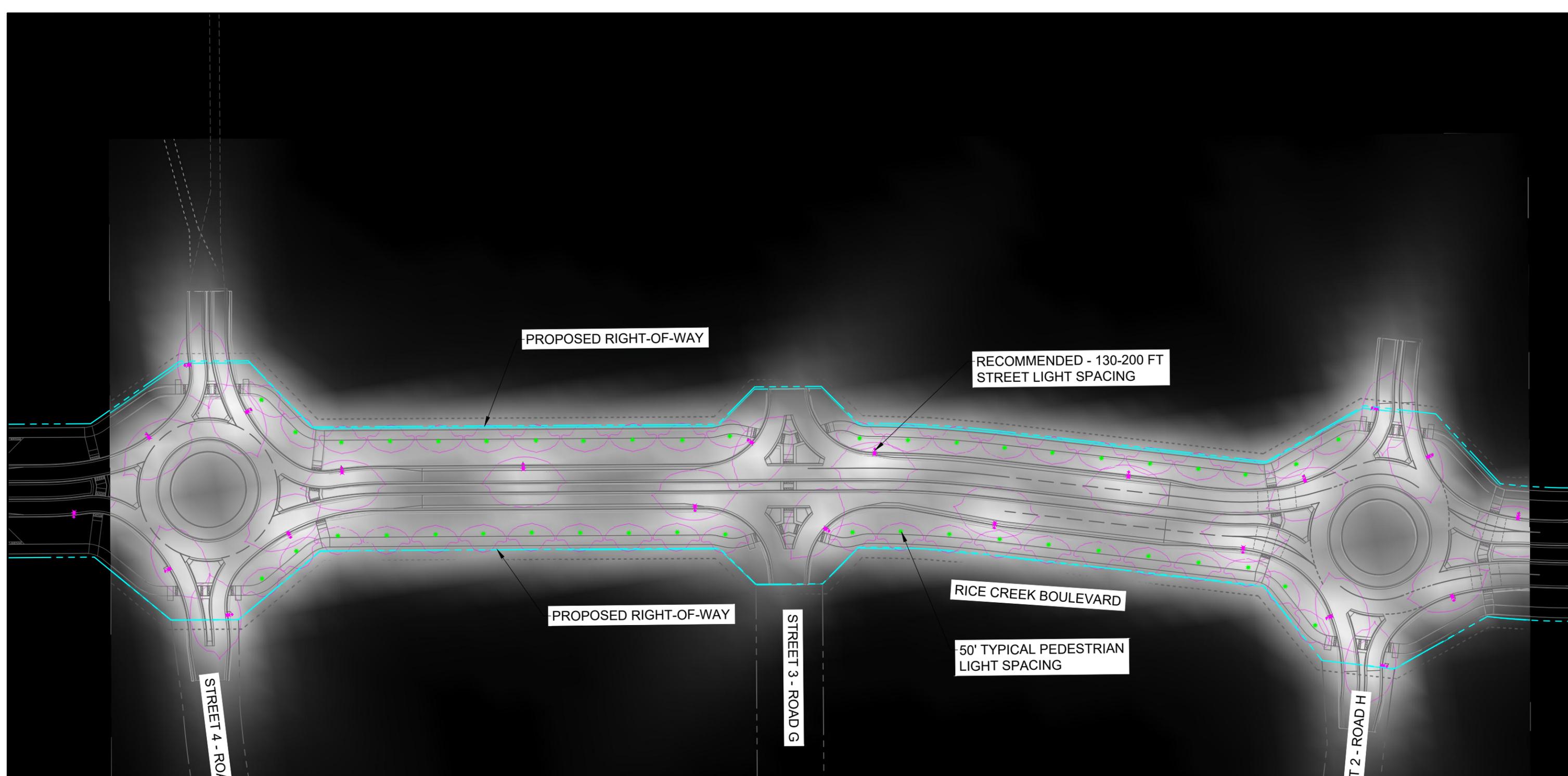
STREET LIGHTS:
 HEIGHT = 30 FT
 POLE STYLE = A - ALUMINUM
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = COBRA
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:



PEDESTRIAN LIGHTS:
 HEIGHT = 18 FT
 POLE STYLE = B - COLUMBUS
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = LANTERN
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:







PEDESTRIAN LIGHT SPACING COMMERCIAL ZONE

LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
	312	4000	30	9360

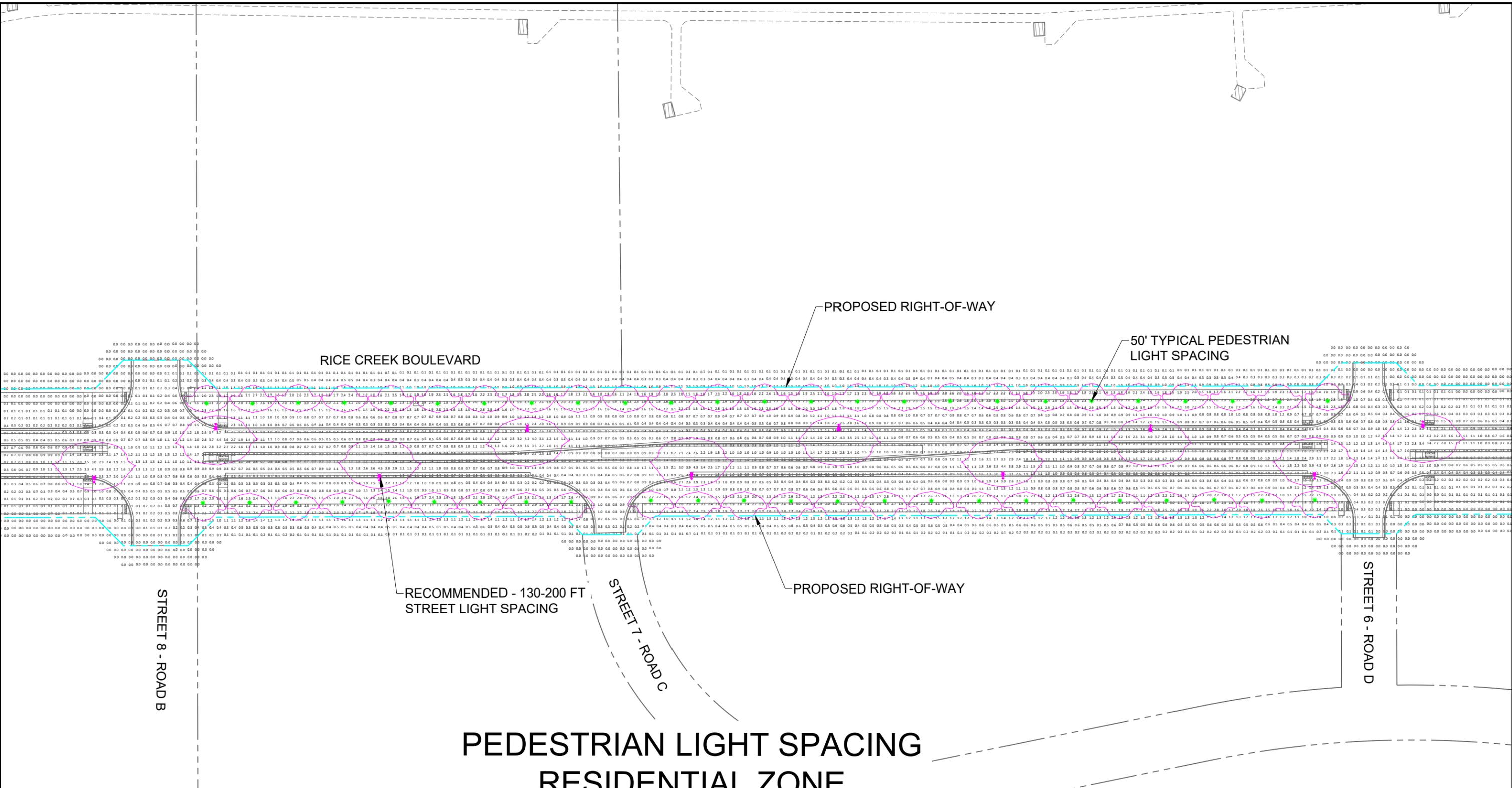
FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.25	4.0	2.50
TRAIL	2.0	2.15	3.0	1.43

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	N/A

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:
 HEIGHT = 30 FT
 POLE STYLE = A - ALUMINUM
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = COBRA
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:

PEDESTRIAN LIGHTS:
 HEIGHT = 18 FT
 POLE STYLE = B - COLUMBUS
 POLE COLOR = BLACK
 LED LUMINAIRE STYLE = LANTERN
 LUMINAIRE COLOR = BLACK
 PLAN SYMBOL:



PEDESTRIAN LIGHT SPACING RESIDENTIAL ZONE

LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
	312	4000	30	9360

FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.07	4.0	2.68
TRAIL	2.0	2.16	3.0	1.44

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				1.9

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:

HEIGHT = 30 FT
POLE STYLE = A - ALUMINUM
POLE COLOR = BLACK

LED LUMINAIRE STYLE = COBRA
LUMINAIRE COLOR = BLACK

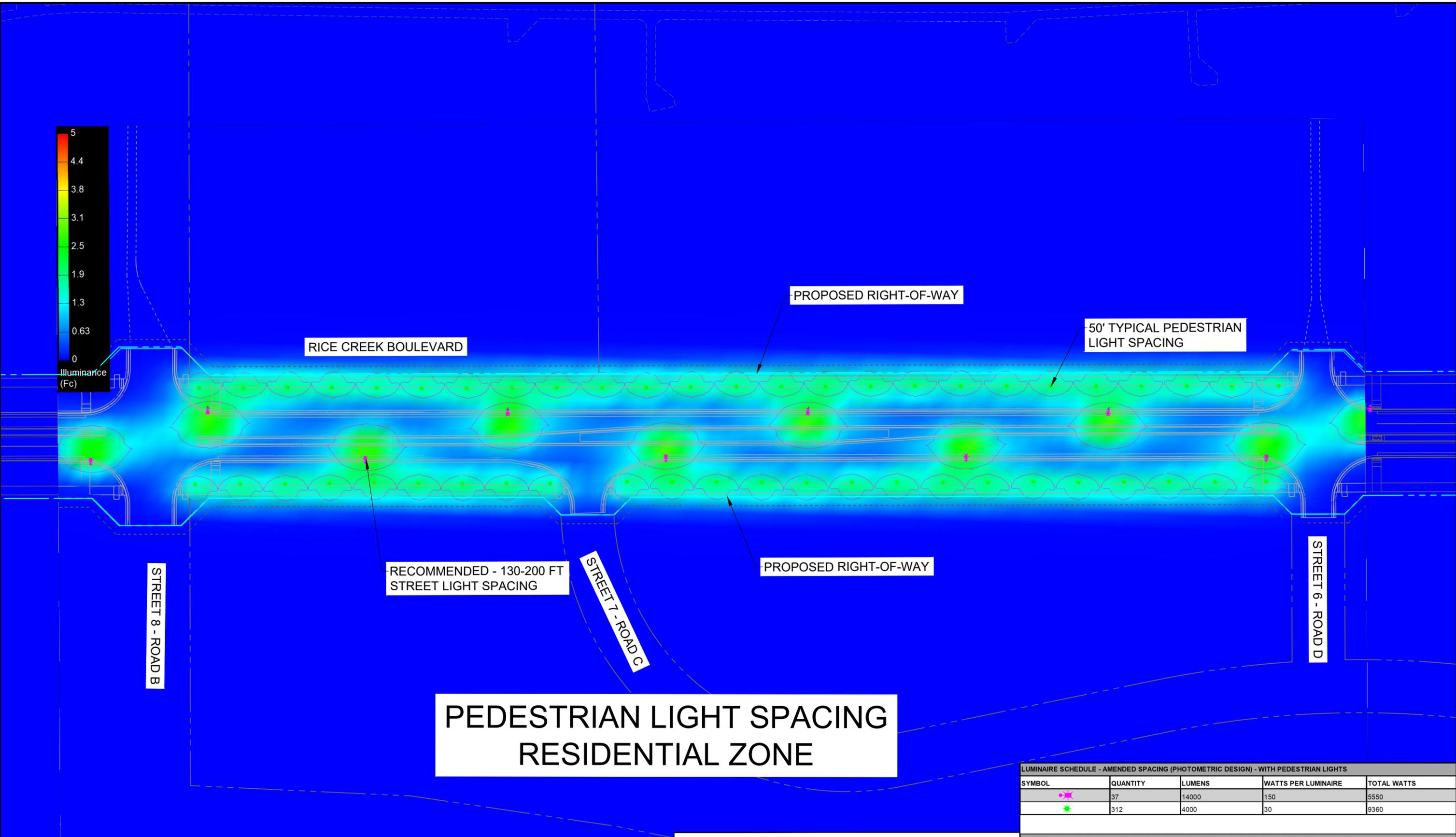
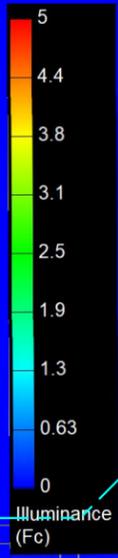
PLAN SYMBOL:

PEDESTRIAN LIGHTS:

HEIGHT = 18 FT
POLE STYLE = B - COLUMBUS
POLE COLOR = BLACK

LED LUMINAIRE STYLE = LANTERN
LUMINAIRE COLOR = BLACK

PLAN SYMBOL:



PEDESTRIAN LIGHT SPACING RESIDENTIAL ZONE

LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
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FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.07	4.0	2.68
TRAIL	2.0	2.16	3.0	1.44

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	1.9

CITY PUBLIC WORKS LIGHTING STYLES

STREET LIGHTS:

HEIGHT = 30 FT
POLE STYLE = A - ALUMINUM
POLE COLOR = BLACK

LED LUMINAIRE STYLE = COBRA
LUMINAIRE COLOR = BLACK

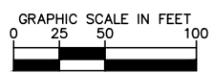
PLAN SYMBOL:

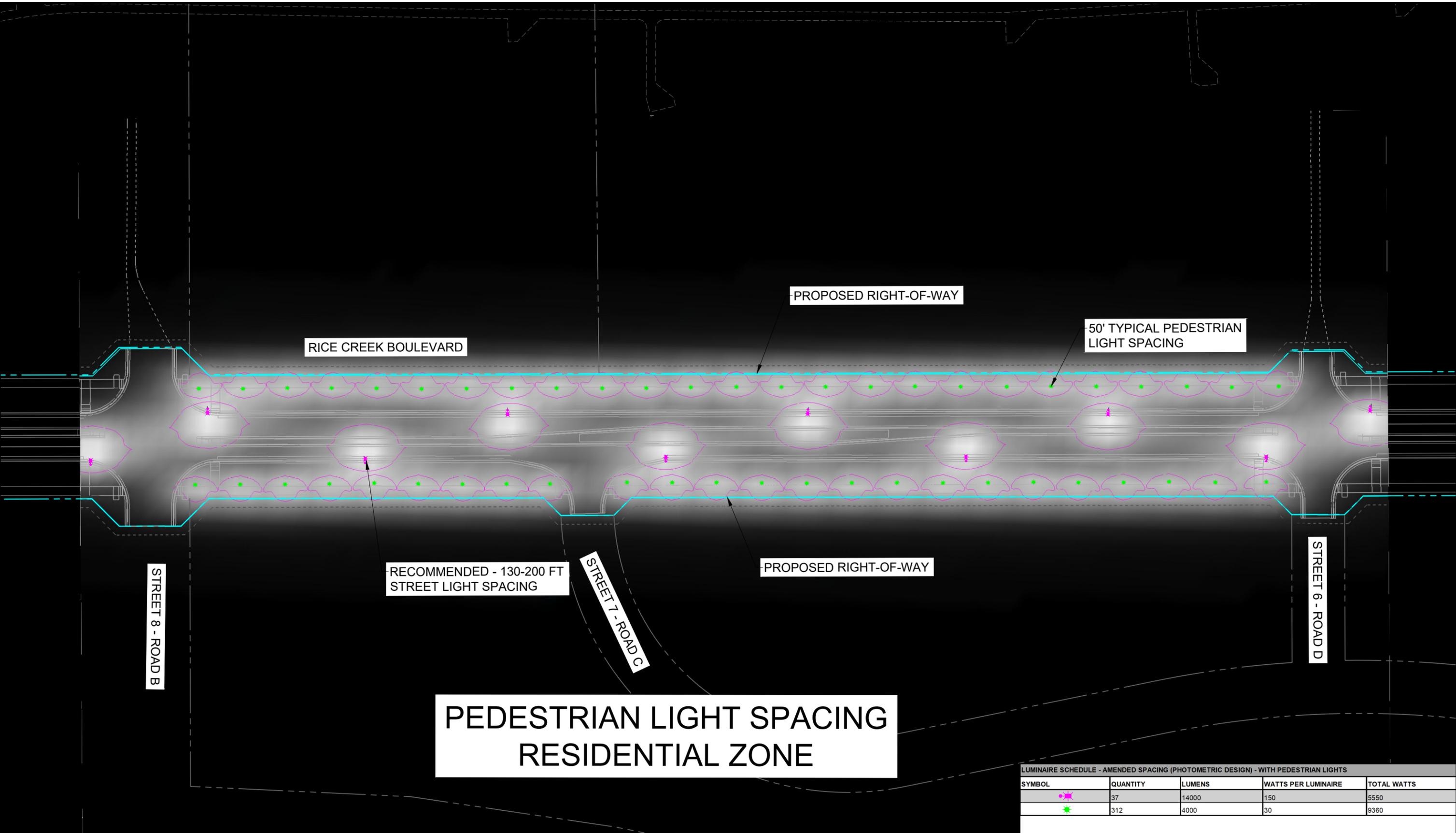
PEDESTRIAN LIGHTS:

HEIGHT = 18 FT
POLE STYLE = B - COLUMBUS
POLE COLOR = BLACK

LED LUMINAIRE STYLE = LANTERN
LUMINAIRE COLOR = BLACK

PLAN SYMBOL:





PEDESTRIAN LIGHT SPACING RESIDENTIAL ZONE

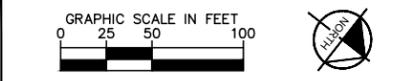
LUMINAIRE SCHEDULE - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
SYMBOL	QUANTITY	LUMENS	WATTS PER LUMINAIRE	TOTAL WATTS
	37	14000	150	5550
	312	4000	30	9360

FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.07	4.0	2.68
TRAIL	2.0	2.16	3.0	1.44

MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)	0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY	1.9

CITY PUBLIC WORKS LIGHTING STYLES

<p>STREET LIGHTS: HEIGHT = 30 FT POLE STYLE = A - ALUMINUM POLE COLOR = BLACK LED LUMINAIRE STYLE = COBRA LUMINAIRE COLOR = BLACK PLAN SYMBOL: </p>	<p>PEDESTRIAN LIGHTS: HEIGHT = 18 FT POLE STYLE = B - COLUMBUS POLE COLOR = BLACK LED LUMINAIRE STYLE = LANTERN LUMINAIRE COLOR = BLACK PLAN SYMBOL: </p>
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RICE CREEK COMMONS

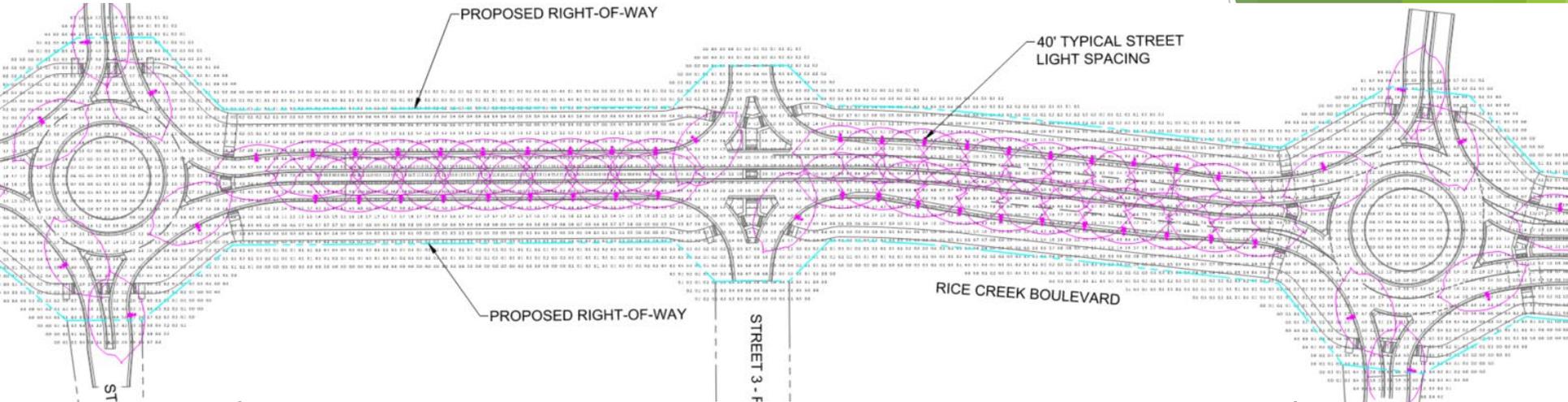
TRC AMENDMENT REQUEST

MARCH 10, 2026

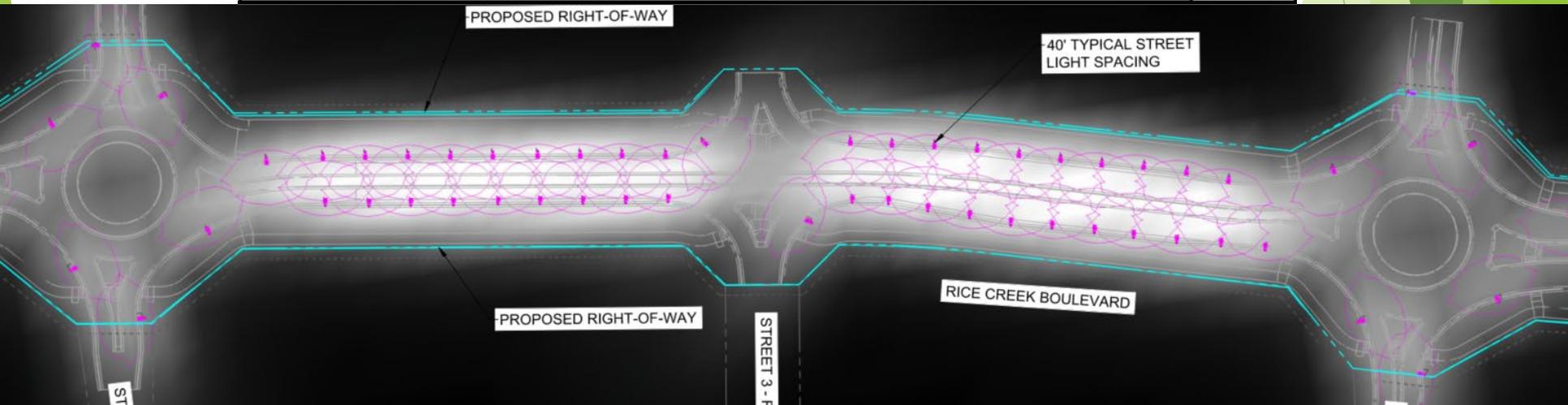
TRC Amendment Item #2: Street Lighting

- ▶ TRC Code: Streetlights at 40-foot average spacing
- ▶ Amendment Request: Allow for context appropriate streetlight spacing using MnDOT Roadway Lighting Design Standards
- ▶ Practical Difficulty:
 - ▶ 40-foot spacing is cost prohibitive
 - ▶ Initial construction and long-term maintenance
 - ▶ 40-foot spacing would result in excessive street lighting
 - ▶ 150-250 foot spacing more typical
- ▶ City of Arden Hills has selected preferred streetlight type and details
- ▶ Applying MnDOT design standards:
 - ▶ Vehicular Lighting: ~200 ft spacing
 - ▶ Pedestrian Lighting: 50-ft spacing

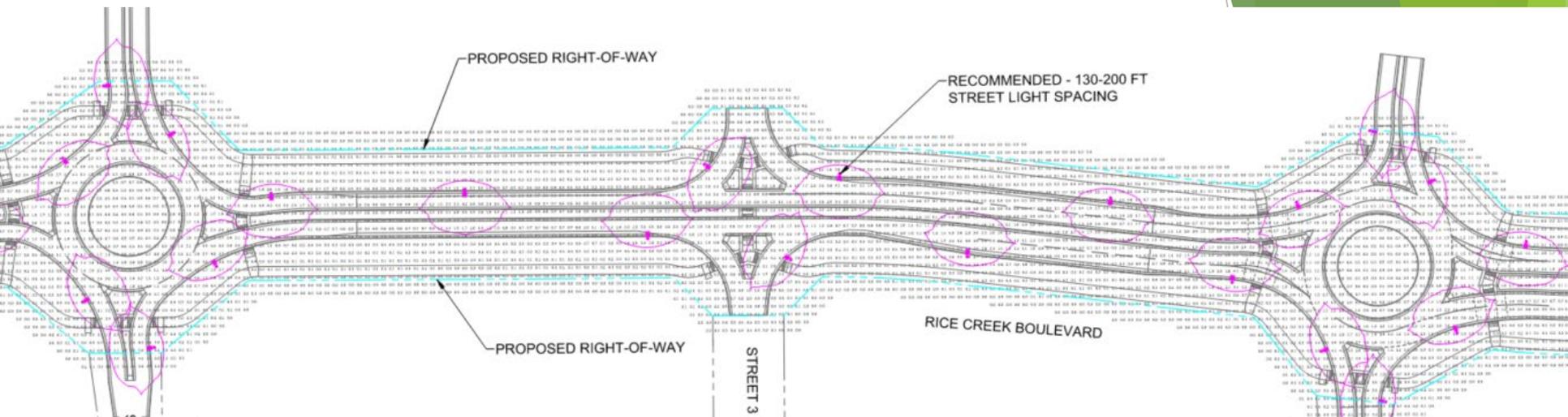
Vehicular Lighting: 40-ft Spacing



FOOT CANDLE LEVELS - 40 SPACING (PER TRC)				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	7.32	4.0	1.44
TRAIL	2.0	0.55	3.0	1.83
MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				N/A

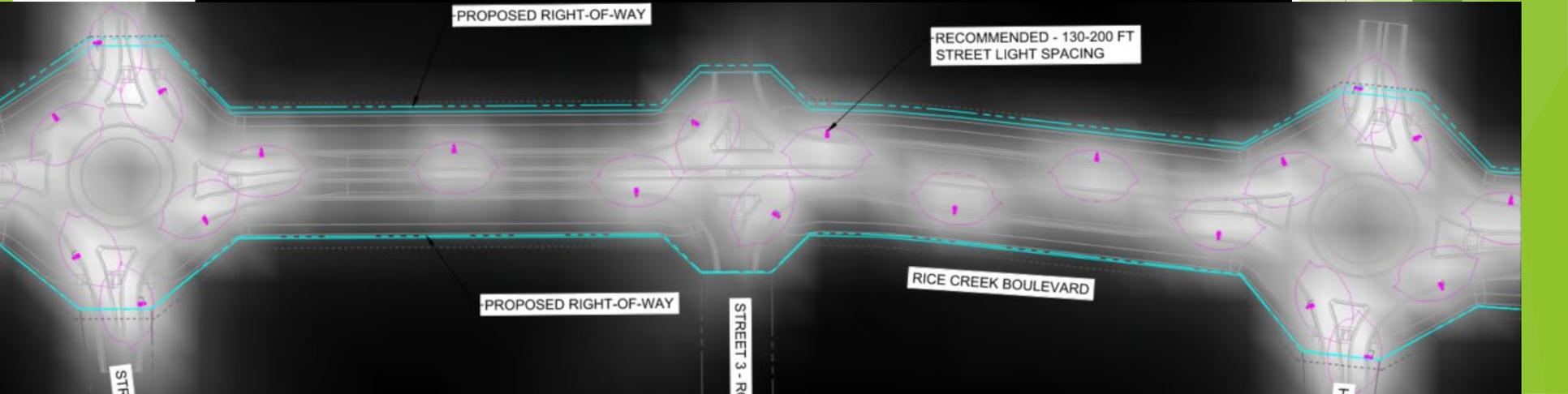


Vehicular Lighting: Proposed Design

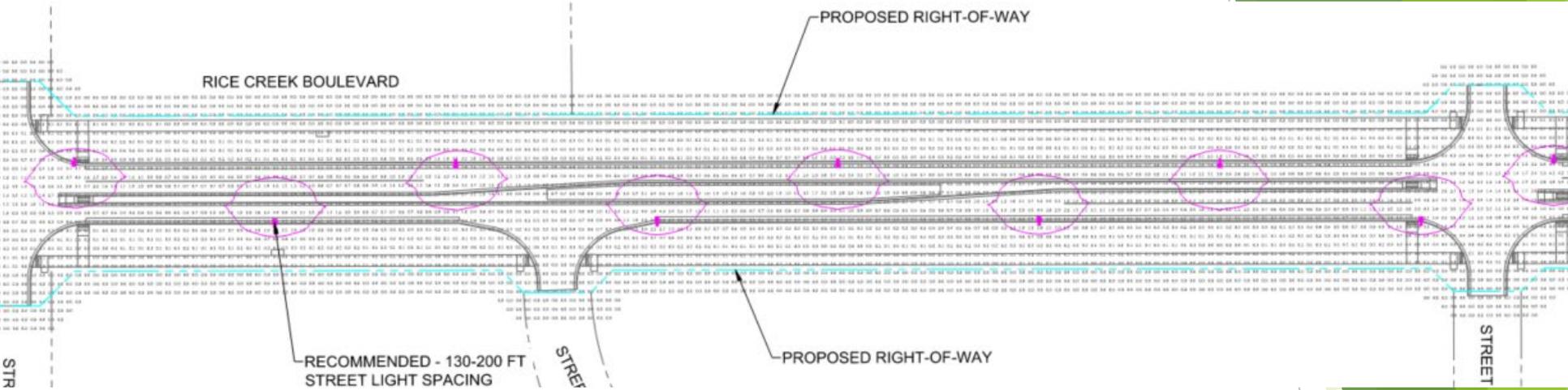


FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN)

LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.20	4.0	3.00
TRAIL	2.0	0.06	3.0	N/A
MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				N/A

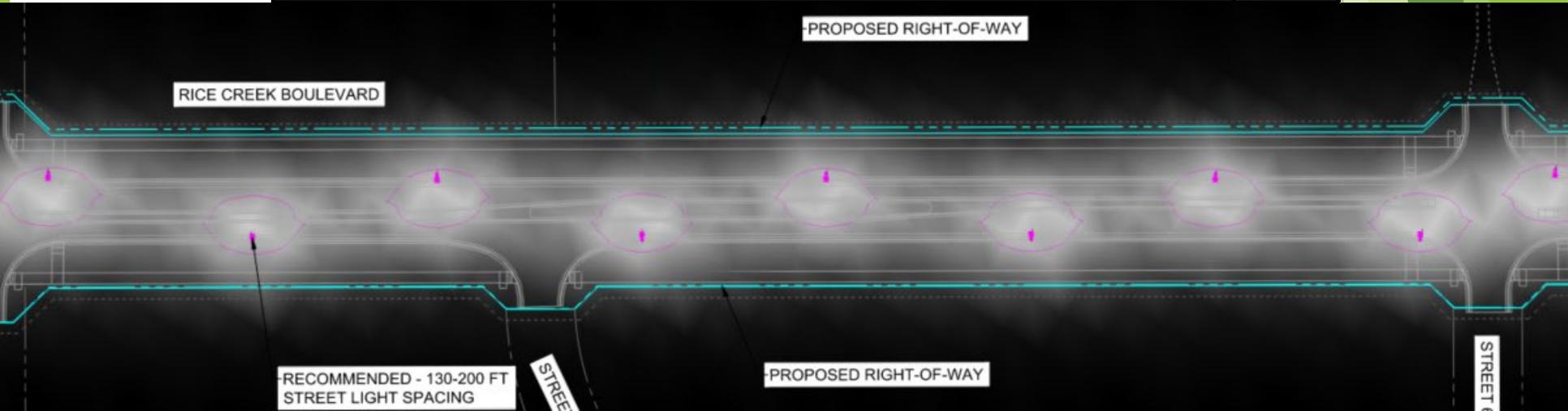


Vehicular Lighting: Proposed Design

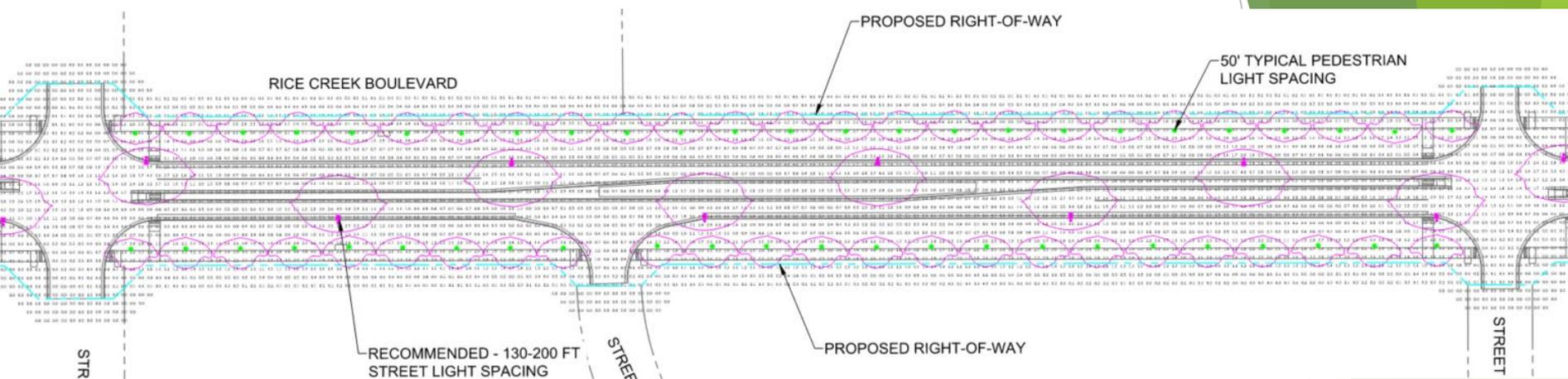


FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN)

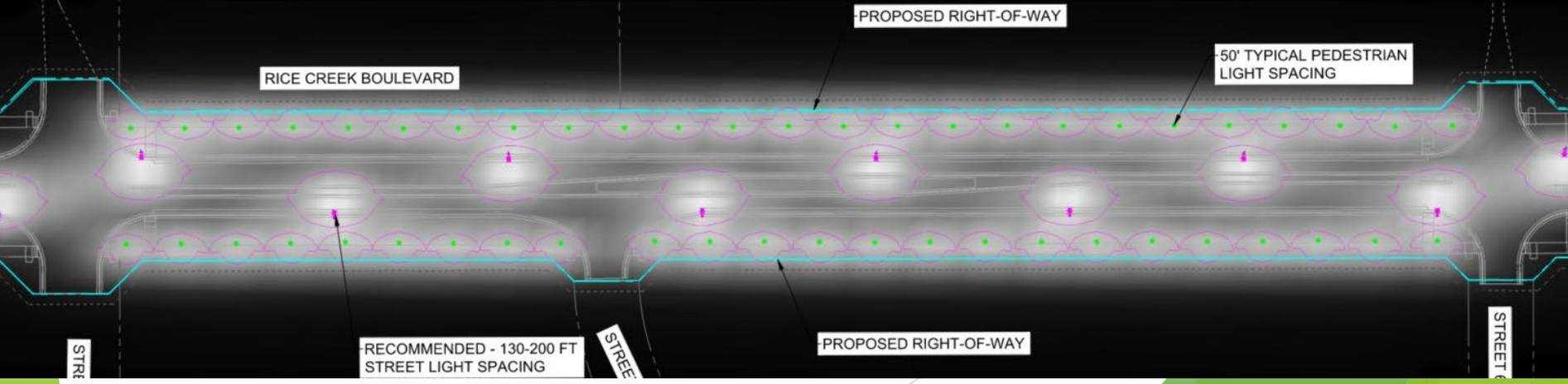
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.05	4.0	3.50
TRAIL	2.0	0.02	3.0	N/A
MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				0.0



Pedestrian Lighting: Potential Design



FOOT CANDLE LEVELS - AMENDED SPACING (PHOTOMETRIC DESIGN) - WITH PEDESTRIAN LIGHTS				
LOCATION	AVG. ILLUMINANCE (Fc) RECOMMENDED	AVG. ILLUMINANCE (Fc) AS DESIGNED	UNIFORMITY (Avg/Min) RECOMMENDED	UNIFORMITY (Avg/Min) AS DESIGNED
ROADWAY	1.0	1.07	4.0	2.68
TRAIL	2.0	2.16	3.0	1.44
MAXIMUM ALLOWABLE ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY (PER CITY CODE 1325.05)				0.4
MAXIMUM ILLUMINANCE AT ADJOINING RESIDENTIAL PROPERTY				1.9



TRC Amendment Process

- ▶ Receive JDA feedback on TRC Amendment request items
 - ▶ February 2nd JDA Work Session
- ▶ **TRC Amendment request for JDA vote/recommendation**
 - ▶ **March 2nd JDA Special Meeting**

Next Steps:

- ▶ Prepare/submit formal amendment request to City Planning Commission
- ▶ City Council formal action on TRC Amendment
- ▶ Ramsey County formal action on TRC Amendment



Joint Development Authority
TCAAP Redevelopment Project



AGENDA ITEM 8b

MEMORANDUM

DATE: March 10, 2026

TO: Joint Development Authority Board of Commissioners

FROM: Directors Mitchell and Jagoe

SUBJECT: Review Annual Report

Per the Joint Powers Agreement (JPA), the JDA must prepare an annual report at the end of each calendar year and submit to the City and County no later than February 15 of the following year. The JDA Annual Report for 2025 was submitted to the City and County and is presented to the JDA for review.

Attachment:
JDA Annual Report 2025

JDA Annual Report 2025

About the Joint Development Authority

In 2012, a Joint Powers Agreement (JPA) between the City of Arden Hills and Ramsey County created the Joint Development Authority (JDA). The JDA oversees the development of the site now known as Rice Creek Commons, a 427-acre piece of land formerly part of the Twin Cities Army Ammunition Plant (TCAAP). The Rice Creek Commons site is located in the City of Arden Hills and is currently owned by Ramsey County.

JDA Membership

The JDA Board consists of two members from the Arden Hills City Council, two members from the Ramsey County Board of Commissioners and one appointed resident from Arden Hills serving as the JDA Chair for a two-year appointment. In 2025, the JDA Board membership was:

- Jonathan Wicklund, Arden Hills Resident, JDA Chair
- Tara Jebens-Singh, Ramsey County Commissioner, JDA Vice Chair
- Mary Jo McGuire, Ramsey County Commissioner
- Tena Monson, Arden Hills City Council member
- Kurt Weber, Arden Hills City Council member

JDA Business

Meetings and Work Sessions

The JDA met approximately monthly in meetings and work sessions to conduct JDA business, listen to public input on the project and take actions to support the development of Rice Creek Commons. The JDA continued its committee work with a JDA Advisory Committee, consisting of Commissioners Jebens-Singh and Monson; and an Energy Advisory Committee, consisting of Commissioners McGuire and Weber. The JDA Advisory Committee met as needed to discuss project updates, provide guidance to staff and recommend topics to bring to JDA meetings and work sessions. The Energy Advisory Committee (EAC) worked with consultants and external partners to advance the Green Energy Vision for the site and make recommendations on sustainability measures to the JDA.

Communications

The JDA worked with Goff Public to communicate about Rice Creek Commons. This included preparing for proactive and reactive opportunities, such as the Outlot A amended purchase and sale agreement and phased development plan (two media stories) and Outlot A phase one groundbreaking (37 media stories).

Additionally, the JDA shared multiple project updates through the Rice Creek Commons e-newsletter and hosted a webinar on sustainability in November. Staff fielded inquiries from the public submitted through the website.

Development Progress

In 2025, the JDA, the City of Arden Hills and Ramsey County made significant steps toward beginning development on Rice Creek Commons.

Outlot A

Outlot A (sometimes called the Thumb Parcel) is the 40-acre portion of Rice Creek Commons along Rice Creek Parkway (formerly Old Highway 8) that is development-ready with infrastructure in place. Ryan Companies has an active purchase agreement and is leading development on this parcel.

In April 2025, Ramsey County, the City of Arden Hills and Ryan Companies broke ground on the new headquarters of Micro Control Company, which manufactures test equipment for the electronics industry. The 157,000-square-foot advanced technology project is the first phase of developing Outlot A. The headquarters will span approximately 10 acres and be home to more than 200 jobs.

The exterior of the building was completed in October. Interior work is underway and is scheduled to be completed in mid-2026.



Micro Control Company headquarters

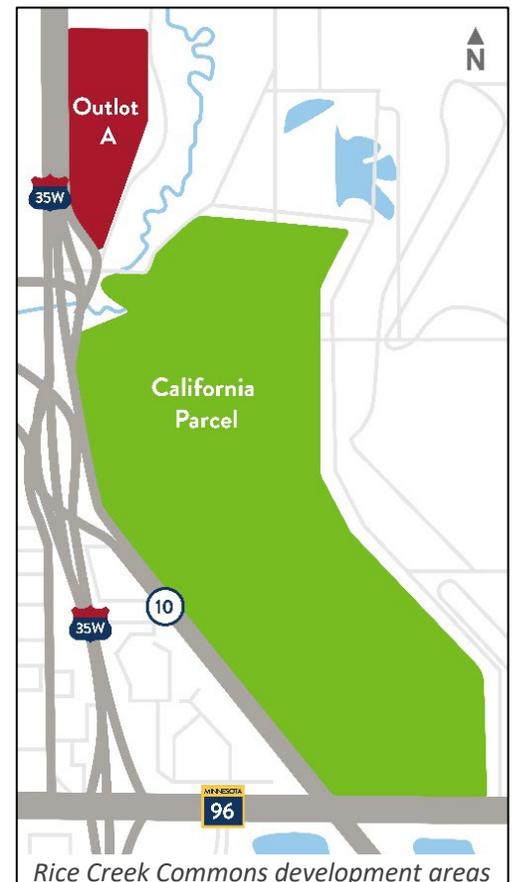
California Parcel

The California Parcel is the larger, 387-acre portion of the development site. This parcel will include a cohesive mix of residential and commercial neighborhoods surrounding a town center — a unique community where people can live, work and play. The site will feature four new neighborhoods offering a mix of single-family homes, townhomes and a walkable retail center with housing, public space and commercial uses. Additionally, the site will be interwoven with parks and trails that will connect into the larger regional system.

The preliminary development agreement between the JDA and Alatus for the California Parcel expired on June 30, 2025. In the fall, the JDA issued a [Request for Information \(RFI\)](#) to gather insights from developers to guide a future competitive solicitation for further development on the California Parcel. The JDA began their discussion of these responses at the December 2025 JDA meeting and will continue these conversations in 2026.

Public Infrastructure

To prepare for private development, Ramsey County has been working with a consultant team led by Kimley-Horn to design public infrastructure on the site. This work includes the design of grading, roadway, drainage, stormwater management features, mitigated wetlands and public utilities associated with Rice Creek Boulevard (previously referred to as the Spine Road). Rice Creek Boulevard will be the main county roadway through Rice Creek Commons providing



access to future residential neighborhoods and commercial areas throughout the site, as well as a direct connection to Interstate 35W.

In September 2025, the JDA Board held a public hearing and approved the final plat for Rice Creek Boulevard, a necessary legal step to dedicate the public right-of-way for the future county road. As part of the plat, two large outlots were created for future development within the California Parcel of Rice Creek Commons, which will be further subdivided and replatted when the time comes for future development. The plat was reviewed by the Minnesota Department of Transportation, the City of Arden Hills and the Rice Creek Watershed District.

The construction of Rice Creek Boulevard is a critical step in enabling future site development, including housing and commercial projects. The final design is anticipated to be completed in August 2026 with initial site work expected to begin in late 2026. Construction, previously estimated to cost approximately \$57 million, will last two full construction seasons.

Clean Energy and Sustainability

The JDA is committed to advocating for clean energy and climate-forward development at Rice Creek Commons. The JDA hosted a public webinar in November 2025 to provide an update on the continued progress toward its sustainability and clean energy goals for the development. The [webinar can be viewed online](#), using the access code 3X!5ruC@.

Sustainability Design Guidelines

In January 2025, the JDA [approved sustainability design guidelines](#) that will help shape the development of the full Rice Creek Commons site. The guidelines include several actionable standards that will further the site's green energy vision and are based on a comprehensive analysis conducted by energy consultant Ever-Green Energy and architecture and engineering firm LHB in 2024.

The standards are based on recommendations presented to the JDA in fall 2024 and memorialized in the Clean Energy Analysis, finalized in November 2024, including guidelines for:

- LEED or similar certification
- Building energy efficiency
- All-electric buildings
- Solar (photovoltaic) systems
- Electric vehicle charging infrastructure
- Energy reporting

Outlot A Sustainability Investments

The Micro Control Company's new headquarters at Outlot A is the first project to be approved under the sustainability guidelines. The building includes a range of sustainability measures including a hybrid ground-source heat pump system as well as rooftop solar, which together mean that the building is almost entirely powered by electricity with just a small amount of natural gas used for back-up heating. By following the sustainability design guidelines, the building is 55% more efficient than baseline and has reduced its carbon emissions by 98.8%.

LEED for Communities

The JDA is pursuing LEED for Communities certification for Rice Creek Commons through the U.S. Green Building Council and submitted for precertification in November 2025. LEED is a globally recognized standard

that establishes benchmarks for energy efficiency and sustainability. LEED for Communities was developed by experts in the built environment industry and provides a comprehensive framework for sustainable urban planning, design, development and long-term improvement. LEED evaluates performance across multiple key areas including natural systems, transportation, water, energy, greenhouse gas reduction, materials, resources and overall quality of life. Achieving this certification will guide the project in meeting its ambitious energy and environmental goals while ensuring accountability through independent third-party verification. It is expected the precertification will be achieved in early 2026 with certification to follow.

Geothermal Grant

Ramsey County, on behalf of the JDA, applied for and received a Geothermal Planning Grant from the Minnesota Department of Commerce in April 2025 to advance geothermal energy planning at Rice Creek Commons. Through this grant, Ever-Green Energy and Darcy Systems have begun work to further investigate the feasibility of ground-source energy for the site. This work will include a geophysical assessment of the site to identify the best locations for geothermal wells and the installation of a test well to gather data. The grant will also support preliminary design work to understand how a potential community district energy system could align with the Rice Creek Boulevard roadway planning. This will further the findings shared in the [Clean Energy Analysis Report](#), which outlines potential options for clean energy solutions to power the site including geothermal, solar and community district energy systems.

Looking Ahead

Looking ahead to 2026, the JDA will continue to advance development opportunities at the Outlot A and California parcels. The Micro Control Center headquarters is slated to open in mid-2026, marking the first completed development at Rice Creek Commons, and the JDA will continue to develop a framework for development on the California parcel. Early site preparation for public infrastructure and Rice Creek Boulevard could begin in late 2026. Rice Creek Commons expects to secure LEED for Communities pre-certification in 2026, with a goal of receiving full certification by early 2027. The JDA will continue to prioritize engaging with the community in 2026, with more details to be shared in the future about upcoming meetings and webinars.

More Information

For more information on the work of the JDA and the development of Rice Creek Commons, please visit:

- [Joint Development Authority webpage](#)
- [Rice Creek Commons project webpage](#)



Joint Development Authority
TCAAP Redevelopment Project



AGENDA ITEM 8c

MEMORANDUM

DATE: March 10, 2026

TO: Joint Development Authority Board of Commissioners

FROM: Directors Mitchell and Jagoe

SUBJECT: Review Road Map

The Road Map has been updated.

Attachment:
2026 JDA Road Map

Rice Creek Commons 2026 Roadmap

Month	Date	Meeting/Action or Deadline	Topics or Notes
Feb	2	JDA Work Session	Infrastructure Updates, Developer Approach
		Special JDA Meeting	Organizational Items
	15	Deadline: JDA Annual Report due to City and County	
Mar	10	Special JDA Meeting	TRC Amendment Requests, Annual Report
	13	JDA Advisory Committee	Hybrid Development Approach
Apr	6	JDA Work Session	
	TBD	Energy Advisory Committee	Sustainability and District Energy Updates
May	1	Deadline: Draft 2027 JDA Budget	
	4	JDA Meeting	Hybrid Development Approach
Jun	1	JDA Work Session	
		Infrastructure Webinar	Timing TBD
Jul	6	JDA Meeting	
Aug	3	JDA Work Session	
Sept	1	Deadline: Coordinate JDA Budget with City and County budget processes	
	15	JDA Meeting	
Oct	5	JDA Work Session	
Nov	2	JDA Meeting	
	15	Deadline: Report back on City/County approval of JDA budget	
Dec	7	JDA Work Session	



Joint Development Authority
TCAAP Redevelopment Project



AGENDA ITEM 9

MEMORANDUM

DATE: March 10, 2026

TO: Joint Development Authority Board of Commissioners

FROM: Director Mitchell

SUBJECT: Administrative Director's Report

A verbal update will be provided by staff.



Joint Development Authority
TCAAP Redevelopment Project



AGENDA ITEM 10

MEMORANDUM

DATE: March 10, 2026

TO: Joint Development Authority Board of Commissioners

FROM: Jake Reilly, City of Arden Hills

SUBJECT: Development Director's Report

A verbal update will be provided by Arden Hills Community Development Director Jake Reilly.